

LAND USE APPLICATION SUMMARY

Property Location: 811 Washington Avenue South, 243, 247, 247 ½, and 249 Chicago Avenue, and 800 ½ 3rd St S

Project Name: Ironclad

Prepared By: Lisa Steiner, Senior City Planner, (612) 673-3950

Applicant: Collage Architects

Project Contact: Josh Jansen

Request: To construct a new mixed use building.

Required Applications:

Conditional Use Permit	To increase the maximum permitted height from 10 stories/140 feet to 14 stories/148 feet
Variance	To reduce the northeast required interior side yard from 15 feet to 4 feet to allow for residential windows on floors three through ten.
Site Plan Review	For a mixed use building with hotel, retail, and 191 dwelling units.

SITE DATA

Existing Zoning	B4N Downtown Neighborhood District DP Downtown Parking Overlay District
Lot Area	71,823 square feet / 1.64 acres
Ward	3
Neighborhood	Downtown East
Designated Future Land Use	Mixed Use
Land Use Features	Mill District Activity Center, Commercial Corridors (Chicago and Washington), Downtown Growth Center
Small Area Plans	<u>Update to the Historic Mills District Master Plan (2001)</u> <u>Downtown East/North Loop Master Plan (2003)</u>

Date Application Deemed Complete	January 26, 2017	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	March 27, 2017	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject property is currently a surface parking lot located at Chicago Avenue and Washington Avenue South. The property was historically used by the Chicago, Milwaukee, and Saint Paul Railway as a connection into the rail facilities in the Mill District and was the site of a grain elevator. In 1884, the company built a viaduct to carry depot-bound trains over Washington Avenue near 8th Avenue South. The viaduct was demolished in 1984, but remnants of the stone wall that supported the viaduct remain along Washington and Chicago Avenues. The site is irregularly shaped due to the former location of the railroad.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The property is surrounded by a mix of uses, including residential units, restaurants, hotels, and offices. Surrounding zoning is mostly B4N Downtown Neighborhood District and C3A Community Activity Center District. There are several surface parking lots in the surrounding area, though some are currently being redeveloped. The site is about one block from the Downtown East light rail station and several bus routes serve the immediate area.

PROJECT DESCRIPTION. The applicant is proposing to construct a mixed use building including 171 dwelling units, a hotel with 148 rooms, approximately 8,000 square feet of retail space, and 388 parking spaces. Additionally, the applicant is proposing a second phase of residential development including approximately 38,000 square feet of additional space and 20 more dwelling units along 9th Avenue South. The initial residential portion would be 14 stories or 148 feet in height. The hotel would be 8 stories or 90 feet tall. The parking spaces would be incorporated in two levels of underground parking and two levels of above grade parking at the rear of the property. This area would be accessed by a curb cut on Chicago Avenue and another curb cut on 9th Avenue South. A valet drop-off would be incorporated at the ground level in the parking structure. Amenities for the hotel and residential uses are proposed above the parking structure at the third floor. The residential portion of the building would be located on the northeast portion of the site along Washington Avenue South and the hotel would be on the northwest side along Chicago Avenue.

The applicant is retaining a significant portion of the existing limestone retaining wall from the historic viaduct, though some deconstruction and reconstruction will be required to allow for construction activities for the building. About 300 linear feet of the wall currently wraps the perimeter of the site along the property lines at Chicago and Washington. Approximately 200 linear feet of the wall would be kept along Chicago and Washington with the proposed new construction. Additional limestone blocks will be utilized as decorative landscape pieces on the site and in the adjacent public right-of-way. Removal of a portion of the wall allows for direct access from the public sidewalk to the retail spaces on Washington Avenue. The proposal also includes an 18 foot wide walkway along the northeast portion of the property that accesses an additional retail space and provides a pedestrian connection from 9th Avenue South to Washington Avenue. Additionally, the applicant is proposing significant improvements to the public realm along 9th Avenue South.

RELATED APPROVALS. None.

PUBLIC COMMENTS. One letter of support was received from the Downtown Minneapolis Neighborhood Association (see appendix). Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow an increase in maximum height in the B4N District from 10 stories or 140 feet to 14 stories, 148 feet based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The maximum allowed height of a building in the B4N Downtown Neighborhood District is 10 stories or 140 feet, whichever is less. The applicant is proposing a building that is 14 stories or 148 feet at the residential portion. The hotel portion of the proposal is only 8 stories in height and would comply with the height requirement. The second phase of residential development would be six stories, including four stories over the structured parking levels. The proposed 14 story, 148 foot tall building will not be detrimental to or endanger the public health, safety, comfort, or general welfare provided that the development complies with all applicable building codes, life safety ordinances, and Public Works standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The property is located in a developed area with a mix of building heights and multi-family residential, commercial, hotel, and office uses. The height of surrounding buildings varies from two stories to towers which are nearly 20 stories. The additional height requested is only on a portion of the site. The additional four stories (8 feet) requested in height should not be injurious to the use or enjoyment of other nearby property and will not impede the development of surrounding property.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The site would be accessed by vehicles on Chicago Avenue and 9th Avenue South. Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Adequate measures will be taken to minimize traffic congestion in the public streets. The applicant is providing 388 parking spaces in a parking structure.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development is located on two designated Commercial Corridors and is within a designated Activity Center and Growth Center. The future land use is identified as Mixed Use. The proposal would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- I.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy I.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- I.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- I.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.
- I.3.3 Encourage above-ground structured parking facilities to incorporate development that provides active uses on the ground floor.

Land Use Policy I.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- I.4.1 Support a variety of commercial districts and corridors of varying size, intensity of development, mix of uses, and market served.
- I.4.2 Promote standards that help make commercial districts and corridors desirable, viable, and distinctly urban, including: diversity of activity, safety for pedestrians, access to desirable goods and amenities, attractive streetscape elements, density and variety of uses to encourage walking, and architectural elements to add interest at the pedestrian level.
- I.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street”.

Land Use Policy I.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- I.5.1 Support an appropriate mix of uses within a district or corridor with attention to surrounding uses, community needs and preferences, and availability of public facilities.

Land Use Policy I.10: Support development along Commercial Corridors that enhances the street’s character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- I.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low-impact light industrial – where compatible with the existing and desired character.
- I.10.2 Encourage commercial development, including active uses on the ground floor, where Commercial Corridors intersect with other designated corridors.
- I.10.5 Encourage the development of high-density housing on Commercial Corridors.

Land Use Policy I.12: Support Activity Centers by preserving the mix and intensity of land uses and by enhancing the design features that give each center its unique urban character.

- I.12.1 Encourage a variety of commercial and residential uses that generate activity all day long and into the evening.
- I.12.2 Encourage mixed use buildings, with commercial uses located on the ground floor and secure entrances for residential uses.
- I.12.3 Encourage active uses on the ground floor of buildings in Activity Centers.
- I.12.6 Encourage the development of high- to very-high density housing within the boundaries of Activity Centers.

Land Use Policy I.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

- I.13.1 Encourage pedestrian-oriented services and retail uses as part of higher density development near transit stations.
- I.13.5 Concentrate highest densities and mixed use development adjacent to the transit station and along connecting corridors served by bus.

Land Use Policy I.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

- I.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

Housing Policy 3.1: Grow by increasing the supply of housing.

- 3.1.1 Support the development of new medium- and high-density housing in appropriate locations throughout the city.

Staff finds that the proposed 14 story, 148 foot height of the building is consistent with the applicable policies of the *Minneapolis Plan for Sustainable Growth*.

- 6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the B4N Downtown Neighborhood District.

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in downtown districts:

- I. *Access to light and air of surrounding properties.*

The subject property fronts on three public streets. This provides adequate separation allowing for access to light and air for surrounding properties to the north, west, and east. The proposal complies with the yard requirements along the south property line as no windows are proposed. The fourteen story portion is between 130 and 160 feet away from the southern property line. Above the tenth floor, the building sets back 18 feet from the northeast property line to allow adequate light and air for the adjacent property to the northeast. Increasing the height of the proposed development should not impede access to light and air that the surrounding properties currently receive.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant has submitted a shadow study identifying shadowing impacts at various times of year. The proposed building would contribute to some shadowing of adjacent properties. The property across Chicago Avenue is currently being redeveloped and would be shadowed by the proposal in spring, fall, and winter mornings. Properties on the north side of Washington Avenue may be shadowed on winter mornings and spring evenings. There are no known solar energy systems that would be shadowed by the proposed development.

3. *The scale and character of surrounding uses.*

The height of surrounding buildings varies from two stories to towers which are nearly 20 stories. The property is surrounded by a mix of uses, including residential units, restaurants, hotels, and offices. Surrounding zoning is mostly B4N Downtown Neighborhood District and C3A Community Activity Center District. There are several surface parking lots in the surrounding area, though some are currently being redeveloped.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The property is located near the Advance Thresher/Emerson-Newton Implement Company building, a local landmark. The requested additional height would be concentrated along Washington Avenue, which should not impact views of the landmark building. The building setback along Chicago Avenue increases towards the Mississippi River as recommended by the applicable small area plan policies. The additional four stories (8 feet) of height requested does not significantly impact views of landmark buildings, significant open spaces, or water bodies, compared to the height permitted by the zoning district.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the northeast required interior side yard from 15 feet to 4 feet on floors three through ten based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the northeast interior side yard setback from 15 feet to 4 feet. In downtown districts, uses are not typically subject to minimum yard requirements. However, residential uses and hotels containing windows which face an interior side yard are subject to a minimum yard equal to five feet plus two feet for every story above the first, up to a maximum of 15 feet. For this building, the minimum yard requirement is therefore 15 feet where residential windows are located along the northeast property line. The windows requiring the variance are located on floors three through ten. If the windows were eliminated, the building could be built up to the property line without requiring a variance.

The property is irregularly shaped due to the former location of the railroad. The site is also uniquely situated in an area with strong policy guidance for increased density and a mix of uses. The site is designated as Mixed Use in the future land use map, is located at the intersection of two Commercial Corridors, is within an Activity Center, and is within the Downtown Growth Center. The site is also only one block from the Downtown East light rail station. The applicant's proposal balances the policies that support increased residential density and a mix of uses. The project is designed to be compatible with surrounding properties. The ground and second floor are set back

over 18 feet from the property line and the uppermost stories are set back 18 feet as well. If windows were not proposed, a building could be constructed right at the property line. Staff finds that practical difficulties existing in complying with the ordinance because of the circumstances unique to the property.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant has proposed to set the windows back 4 feet from the interior side property line on floors three through ten. In general, yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. The urban design policies of the comprehensive plan also address this issue in calling for building placement to allow light and air into the site and surrounding properties. The building is designed in a manner that would allow for adequate access to light and air for the subject site and adjacent property to the northeast. The immediately adjacent use is a surface parking lot with a building between 80 and 160 feet from the shared property line. If the windows were eliminated, the proposed building could be built up to the property line. The applicant proposes to use the property in a reasonable manner by incorporating residential windows and the proposal will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The applicant has demonstrated that the development, as proposed, would provide sufficient light and air for the adjacent uses while maintaining a mix of uses that is characteristic of the surrounding properties. The proposed windows would be between 80 and 160 feet from the nearest building. The proposed setback variance would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

1. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – Requires alternative compliance

- The first floor of the building is located within eight feet of the front lot line on Washington Avenue. The first floor is located between 9 and 20 feet from the front lot line on Chicago Avenue. This requires alternative compliance.
- The placement of the building reinforces the street wall, maximizes natural surveillance and visibility, and facilitates pedestrian access and circulation. Additionally, the applicant is retaining a significant portion of the existing retaining wall while providing direct access from the street along Washington Avenue for the proposed retail spaces.

- The area between the building and lot line includes amenities like a patio, landscaping, and the rebuilt historic retaining wall.
- All on-site accessory parking is located to the rear or interior of the site within two levels of underground parking and two levels of above-grade parking.

Principal entrances – Meets requirements

- The building is oriented so that at least one principal entrance faces the front property line on Chicago Avenue and Washington Avenue.
- All principal entrances are clearly defined and emphasized through the use of architectural features.

Visual interest – Requires alternative compliance

- With the exception of the blank walls described below, the building walls provide architectural detail and contain windows in order to create visual interest.
- The proposed building emphasizes architectural elements – including recesses, projections, windows, and entries – to divide the building into smaller identifiable sections.
- There are two blank, uninterrupted walls exceeding 25 feet in length on the south elevation of the building. This requires alternative compliance.

Exterior materials – Meets requirements

- The applicant is proposing brick, metal panel, and glass as the building's exterior materials. Each elevation would comply with the City's durability standards for exterior materials. Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- In addition, the application is consistent with the City's policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.
- The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.

Windows – Requires alternative compliance

- For residential uses, the zoning code requires that no less than 20 percent of the walls on the first floor, and no less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. The project is in compliance with the minimum residential window requirement (see Table I).
- For nonresidential uses, the zoning code requires that no less than 30 percent of the walls on the first floor are windows with clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher. No less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. The project is in compliance with the minimum window requirement on Chicago Avenue and Washington Avenue but not on 9th Avenue (see Table I). Alternative compliance is required. Additionally, the applicant has not indicated the visual light transmittance ratio of the ground floor nonresidential windows. A condition of approval is recommended requiring a visible light transmittance ratio of six-tenths (0.6) or higher.
- With the exception of the windows at the thirteenth and fourteenth floor of the residential portion and the eighth floor of the hotel, all windows are vertical in proportion and are evenly distributed along the building walls. Alternative compliance is required for the horizontal windows.

Table 1. Percentage of Windows per Applicable Elevation

	Code Requirement		Proposed	
Residential Uses				
1st floor: Washington	20% minimum	56 sq. ft.	52%	145 sq. ft.
2nd floor and above: Washington	10% minimum	164 sq. ft.	46%	768 sq. ft.
2 nd floor and above: Chicago	10% minimum	57 sq. ft.	62%	348 sq. ft.
2 nd floor and above: 9 th Avenue	10% minimum	125 sq. ft.	44%	552 sq. ft.
Nonresidential Uses				
1st floor: Washington	30% minimum	324 sq. ft.	49%	526 sq. ft.
1st floor: 9 th Avenue	30% minimum	312 sq. ft.	3%	32 sq. ft.
1st floor: Chicago	30% minimum	564 sq. ft.	52%	980 sq. ft.
2nd floor and above: Chicago	10% minimum	174 sq. ft.	38%	660 sq. ft.
2 nd floor and above: 9 th Avenue	10% minimum	164 sq. ft.	0%	0 sq. ft.

Ground floor active functions – Requires alternative compliance

- At least 70 percent of the first floor building frontage facing the public streets is required to contain active functions. The ground floor facing Chicago Avenue contains 80 percent (188 feet) active functions. The ground floor facing Washington Avenue contains 100 percent (152 feet) active functions. The ground floor facing 9th Avenue South contains 16 percent (18 feet) active functions. Alternative compliance is required for the elevation facing 9th Avenue South.

Roof line – Meets requirements

- The principal roof line of the building will be flat, which is similar to that of surrounding buildings.

Parking garages – Requires alternative compliance

- All of the proposed parking is located below ground or enclosed within the parking structure portion of the building.
- Sloped floors do not dominate the appearance of the walls on the parking garage, and vehicles would be screened from view.
- The proposed parking garage does not comply with the minimum windows and ground floor active functions requirements. Alternative compliance is required.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- There are clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.

Transit access – Not applicable

- No transit shelters are proposed as part of this development.

Vehicular access – Meets requirements

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses.
- Curb cuts have been consolidated. There are currently two curb cuts on Chicago Avenue and direct access to the parking area on 9th Avenue South. The proposal would include one curb cut on Chicago and one curb cut on 9th Avenue South.
- There are no public alleys adjacent to the site.
- Service vehicle access does not conflict with pedestrian traffic on public right-of-way.
- There is no maximum impervious surface requirement in the B4N zoning district. According to the materials submitted by the applicant, 91 percent of the site will be impervious, while 93 percent of the existing site is impervious.

LANDSCAPING AND SCREENING

General landscaping and screening – *Not applicable*

- In the Downtown Districts, any building containing 50,000 square feet or more of gross floor area is exempt from the general landscaping and screening requirements.

Parking and loading landscaping and screening – *Not applicable*

- There is no surface parking proposed for the site, so the site is not subject to the screening and landscaping requirements for parking areas per section 530.170.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – *Not applicable*

- All parking is located within a parking structure.

Site context – *Meets requirements*

- There are no parks, greenways, significant buildings, or water bodies near the site that will be obstructed by the proposed building.
- As noted in the conditional use permit analysis, this building should have minimal shadowing effects on public spaces and adjacent properties.
- This building has been designed to minimize the generation of wind currents at ground level.

Crime prevention through environmental design – *Meets requirements*

- The site plan employs best practices to increase natural surveillance and visibility, to control and guide movement on the site, and to distinguish between public and non-public spaces.
- The proposed site, landscaping, and buildings promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- The project provides lighting on site, at all building entrances, and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.
- The landscaping, sidewalks, lighting, fencing, and building features are located to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- The entrances, exits, signs, fencing, landscaping, and lighting are located to distinguish between public and private areas, to control access, and to guide people coming to and going from the site.

Historic preservation – Meets requirements with Conditions of Approval

- This site is neither historically designated nor is it located in a historic district.
- The development would include the partial reuse of a significant historic feature, the remaining historic retaining wall from the railroad viaduct that was once located on site. The applicant has indicated that deconstruction and reconstruction may be required so as not to damage the wall during construction activities for the building. Because about 100 linear feet of the wall would be removed along Washington Avenue, staff recommends a condition of approval that the northeast corner portion of the wall be retained to delineate the historic corner of this wall that follows the irregular angled shape of the property line. Additionally, to assist with interpretation of the wall, staff recommends a condition of approval that the applicant shall install one or more interpretive signs on site that explains the history of the wall and its former use.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed uses are permitted in the B4N District.

Off-street Parking and Loading – Meets requirements

- There is no minimum parking requirement for residential uses in the downtown districts, except that multi-family dwellings that provide parking must provide one designated visitor space for each 50 dwelling units, so considering the 191 dwelling units in the first and second phases of residential development, 4 visitor spaces are required. The maximum parking requirement for the residential uses would be 301 spaces. The combined maximum parking requirements for the hotel, retail, and restaurant uses would be 178 spaces. The proposal meets the parking requirements (see Table 2).
- The minimum bicycle parking requirement is 99 spaces. The applicant is providing 211 bike parking spaces (see Table 3).
- The off-street loading requirement is one small space for the residential uses and two large spaces for the hotel use (see Table 4). One large loading space is accommodated near the end of 9th Avenue South. The small loading space and an additional large loading space are on the first floor of the parking structure.

Table 2. Vehicle Parking Requirements Per Use (Chapter 54I)

Use	Minimum	Maximum Allowed	Proposed
Residential Dwellings	0; 4 visitor	301	232
Commercial recreation, entertainment and lodging	0	178	156
Retail sales and services			
Food and beverages			
	0 spaces 4 visitor spaces	479	388

Table 3. Bicycle Parking Requirements (Chapter 54I)

Use	Minimum	Short-Term	Long-Term	Proposed
General Retail Sales and Services	3	Not less than 50%	--	40
Residential Dwellings	96	--	Not less than 90%	171
	99	--	--	211

Table 4. Loading Requirements (Chapter 54I)

Use	Loading Requirement	Minimum Requirement	Proposed
Restaurant, sit down	Low	None	None
General Retail Sales and Services	Low	None	None
Residential Dwellings	One small	One small	One small
Hotel	Medium	Two large	Two large
	--	One small, two large	One small, two large

Building Bulk and Height – Requires conditional use permit

- The applicant has requested a conditional use permit to increase the height of the building from 10 stories or 140 feet to 14 stories, 148 feet. There is no maximum floor area ratio in the B4N District. The floor area for the second phase of residential development is included in the calculation. The proposal meets the minimum floor area requirement of 2.0 (see Table 5).

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	71,823 square feet / 1.64 acres
Gross Floor Area	--	288,463 sq. ft.
Floor Area Ratio (Minimum)	2.0	4.02
Floor Area Ratio (Maximum)	None	
Building Height (Maximum)	10 stories or 140 feet, whichever is less	14 stories/148 feet

Lot Requirements – Not applicable

Yard Requirements – Requires variance(s)

- In downtown districts, uses are not typically subject to minimum yard requirements. However, residential uses and hotels containing windows which face an interior side yard are subject to a minimum yard equal to five feet plus two feet for every story above the first, up to a maximum requirement of 15 feet. For this fourteen-story building, the minimum yard requirement is therefore 15 feet where windows are located along the northeast and south property lines. The applicant is proposing to locate residential windows 4 feet from the northeast property line which requires a variance. No hotel windows are proposed on the south elevation within the required yard.

Signs – Meets requirements

- All signs are subject to Chapter 543, On-Premise Signs. No specific signage is proposed at this time. The applicant will be required to submit a separate sign permit application for any signage that is proposed.

Screening of Mechanical Equipment – Meets requirements

- All mechanical equipment is subject to the screening requirements of [Chapter 535](#) and district requirements. The applicant has indicated that all mechanical equipment will be screened according to the standards in the zoning code.

Refuse Screening – Meets requirements

- All refuse and recycling storage containers are located within the building.

Lighting – Meets requirements

- Existing and proposed lighting must comply with [Chapter 535](#) and Chapter 54I of the zoning code. No photometric plan has been submitted at this time. All lighting will be reviewed prior to the issuance of any permits to verify compliance with the requirements above.

Fences – Not applicable

Specific Development Standards – Meets requirements

- The applicant's proposal will meet the specific development standards for hotels, multiple-family dwellings, and restaurants in [Chapter 536](#).

DP Overlay District Standards – Meets requirements

- The proposal is in compliance with the DP Downtown Parking Overlay District standards.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as Mixed Use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan, in addition to the policies noted above in the conditional use permit findings:

Heritage Preservation Policy 8.1: Preserve, maintain, and designate districts, landmarks, and historic resources which serve as reminders of the city's architecture, history, and culture.

- 8.1.3 Encourage new developments to retain historic resources, including landscapes, incorporating them into new development rather than removal.

Urban Design Policy 10.1: Promote building designs and heights that enhance and complement the image and form of the Downtown skyline, provide transition to the edges of Downtown and protect the scale and quality in areas of distinctive physical or historical character.

- 10.1.1 Concentrate the tallest buildings in the Downtown core.
- 10.1.2 Building placement should preserve and enhance public view corridors that focus attention on natural or built features, such as landmark buildings, significant open spaces or water bodies.
- 10.1.3 Building placement should allow light and air into the site and surrounding properties.

Urban Design Policy 10.2: Integrate pedestrian scale design features into Downtown site and building designs and infrastructure improvements.

- 10.2.1 The ground floor of buildings should be occupied by active uses with direct connections to the sidewalk.

- 10.2.2 The street level of buildings should have windows to allow for clear views into and out of the building.
- 10.2.3 Ensure that buildings incorporate design elements that eliminate long stretches of blank, inactive building walls such as windows, green walls, architectural details, and murals.
- 10.2.4 Integrate components in building designs that offer protection to pedestrians, such as awnings and canopies, as a means to encourage pedestrian activity along the street.
- 10.2.5 Locate access to and egress from parking ramps mid-block and at right angles to minimize disruptions to pedestrian flow at the street level.
- 10.2.8 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

- 10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.3 Large-scale, high-rise, multi-family residential development is more appropriate in the Downtown Minneapolis Growth Center.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.
- 10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.
- 10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.
- 10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Urban Design Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development.

- 10.9.1 Encourage both mixed-use buildings and a mix of uses in separate buildings where appropriate.
- 10.9.2 Promote building and site design that delineates between public and private spaces.
- 10.9.3 Provide safe, accessible, convenient, and lighted access and way finding to transit stops and transit stations along the Primary Transit Network bus and rail corridors.
- 10.9.4 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

- 10.18.5 Design parking structures so sloping floors do not dominate the appearance of the walls.
- 10.18.6 The ground floor of parking structures should be designed with active uses along the street walls except where frontage is needed to provide for vehicular and pedestrian access.
- 10.18.7 Minimize the width of ingress and egress lanes along the public right of way in order to provide safe pedestrian access across large driveways.
- 10.18.8 Encourage appropriate land uses to share parking lots to reduce the size and visual impact of parking facilities.

CPED finds that with the recommended conditions of approval, the proposed development is in conformance with the above policies of *The Minneapolis Plan for Sustainable Growth*.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of the *Update to the Historic Mills District Master Plan* adopted by the City Council in 2001 and the *Downtown East/North Loop Master Plan* adopted in 2003. The *Historic Mills District* plan envisioned the future land use for this site as ground floor retail with housing or commercial uses above and a proposed 500-car parking garage with a plaza above. The recommended height was 8 or 9 stories. The plan also emphasized the importance of Chicago Avenue to link the riverfront to downtown. The *Downtown East/North Loop Master Plan* identified the future land use for the site as Mixed Use – Residential with a height between 5 and 13 stories. At-grade retail was recommended at Washington & Chicago, and Chicago Avenue was identified as a primary pedestrian corridor. The proposal is in conformance with the applicable plans adopted by the City Council.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

Building placement. The first floor is located between 9 and 20 feet from the front lot line on Chicago Avenue, which requires alternative compliance. As proposed, this meets guidance in the *Update to the Historic Mills District Master Plan*, which recommends a gradually increasing setback for the east side of Chicago Avenue from the Mississippi River southward. The main entrance to the hotel on Chicago extends out to 5 feet from the property line on Chicago. Overall, the proposal meets the intent of the applicable plans and granting alternative compliance is recommended.

Visual interest. There are two blank, uninterrupted walls exceeding 25 feet in length on the south elevation of the building. There is a 33 foot wide blank wall at the southwest corner of the building that is adjacent to a surface parking area. There is also a 277 foot wide blank wall that would be visible from the adjacent property, which has several windows and a landscaped area facing this wall. Staff does not recommend granting alternative compliance for these blank walls as no mitigating measures are proposed. Therefore, a condition of approval is recommended which requires no blank, uninterrupted walls exceeding 25 feet in length on the south elevation.

Windows. The project is in compliance with the minimum window requirement on Chicago Avenue and Washington Avenue but not on 9th Avenue. The proposal includes only 32 square feet of windows along 9th Avenue South, where 312 square feet would be required. However, when including the glass

pedestrian doors and the transparent garage door, 291 square feet of glass is proposed along this elevation. This generally meets the intent of the requirement which is to allow for views into and out of the building. The site faces three public streets and is well in excess of the window requirement on the other street-facing facades. Staff therefore recommends granting alternative compliance.

Additionally, the windows at the thirteenth and fourteenth floor of the residential portion and the eighth floor of the hotel are horizontal in proportion, which requires alternative compliance. The majority of the windows on the project are vertically proportioned and the horizontal windows provide design differentiation. Staff recommends granting alternative compliance.

Ground floor active functions. The ground floor facing 9th Avenue South contains only 16 percent (18 feet) active functions where 70 percent is required. The site fronts on three public streets. Focusing active uses along Chicago and Washington is supported by small area plan policies and comprehensive plan policies which designate Chicago and Washington as Commercial Corridors. The proposal also provides active uses along the interior side yard and a pedestrian connection between 9th Avenue South and Washington Avenue which would not be required through site plan review. Staff recommends granting alternative compliance, with a condition that the ornamental grasses indicated on the plans and some perennial plantings be planted along this wall to improve the aesthetic of the inactive portions along the public street.

Parking garages. As noted and analyzed above, the proposed parking garage does not comply with the minimum windows and ground floor active functions requirements along 9th Avenue South. Staff recommends granting alternative compliance with conditions of approval as noted above.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Collage Architects for the properties located at 811 Washington Ave S, 243, 247, 247 ½, and 249 Chicago Ave, and 800 ½ 3rd St S:

A. Conditional Use Permit to increase maximum height.

Recommended motion: **Approve** the application to increase maximum height from 10 stories or 140 feet to 14 stories, 148 feet, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Variance to reduce the northeast interior side yard requirement.

Recommended motion: **Approve** the application for a variance to reduce the northeast interior side yard requirement from 15 feet to 4 feet for residential windows on floors three through ten.

C. Site Plan Review.

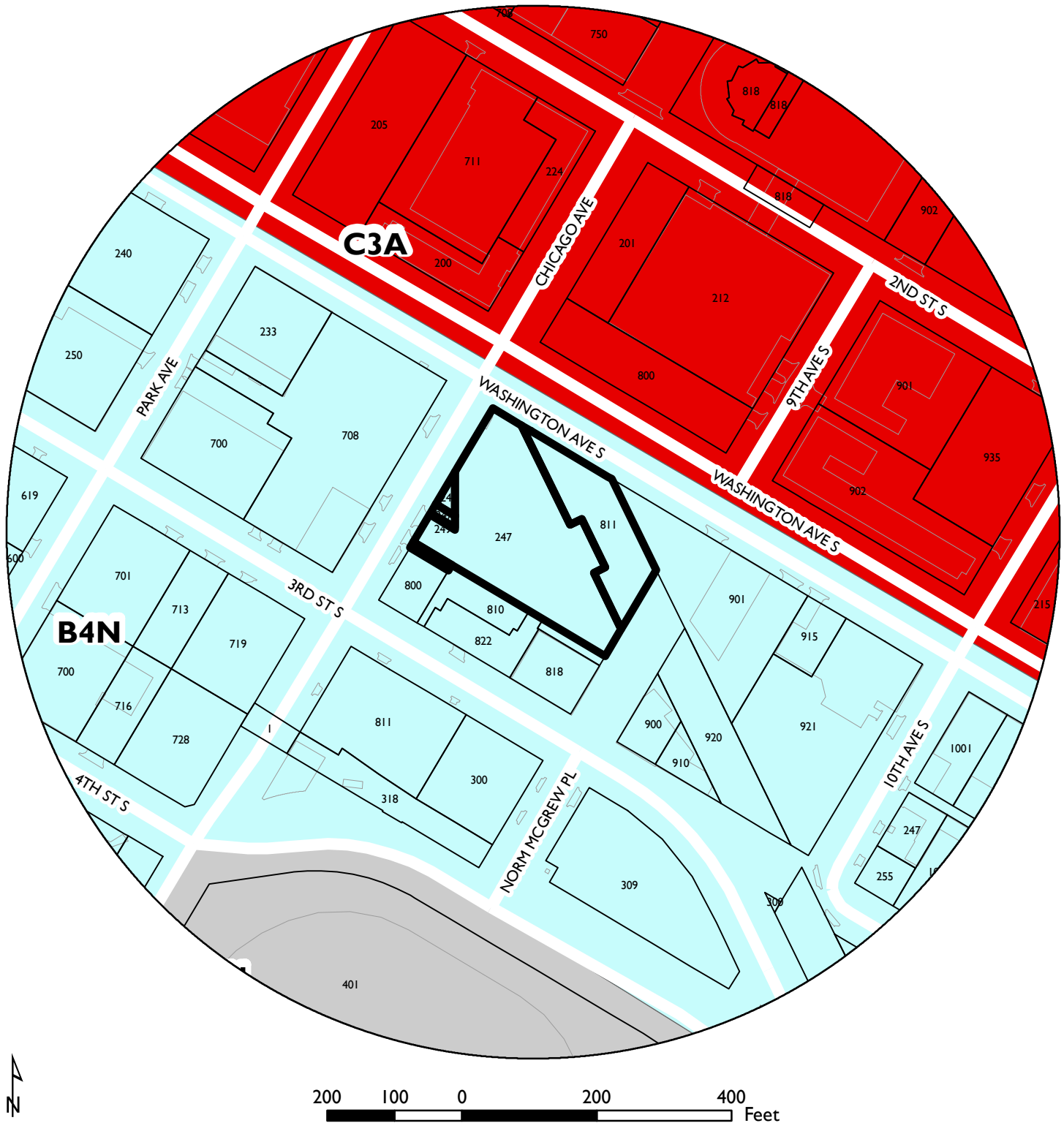
Recommended motion: **Approve** the application for a new mixed use building, subject to the following conditions:

1. All site improvements shall be completed by February 21, 2019, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.

3. Blank, uninterrupted walls on the south elevation shall not exceed twenty-five (25) feet in length, in compliance with section 530.120 of the zoning code.
4. The first floor windows of the nonresidential portions of the building shall have clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher, in compliance with section 530.120 of the zoning code.
5. The historic retaining wall shall be incorporated along Chicago Avenue and part of Washington Avenue South as indicated in the plans. In addition, the northeast corner of the retaining wall shall be incorporated to delineate the historic corner of the wall in order to meet the intent of section 530.270 of the zoning code. The applicant also shall incorporate one or more interpretive signs on site to describe the history of the property.
6. Ornamental grasses and perennials shall be planted along the inactive portions of the building wall facing 9th Avenue South.

ATTACHMENTS

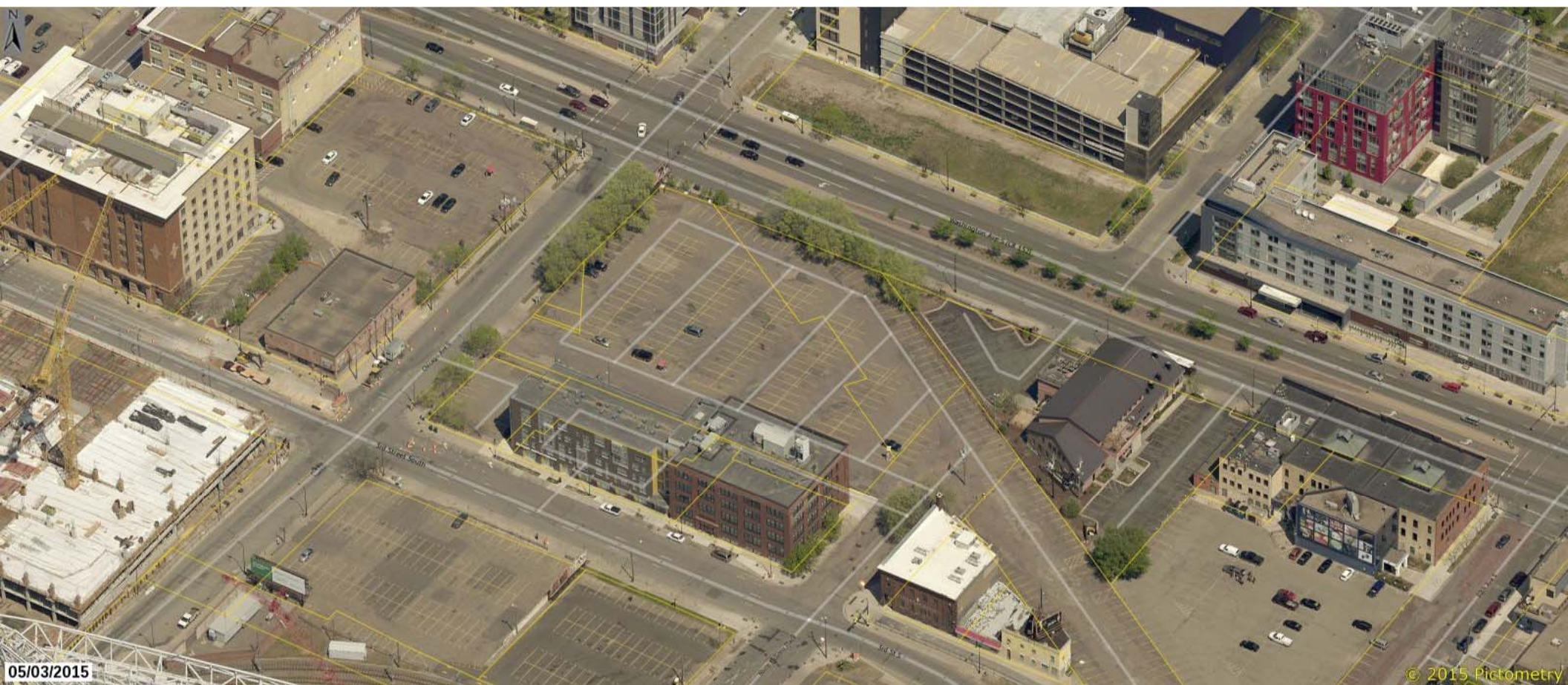
1. Zoning map
2. Oblique aerial photo
3. Historic photos of viaduct
4. Sanborn map - 1930
5. Written description and findings submitted by applicant
6. Renderings
7. Photos
8. Plans and elevations
9. Shadow study
10. Public comments



PROPERTY ADDRESS

FILE NUMBER

PLAN3636



Viaduct at Washington & Chicago - Historic Photos

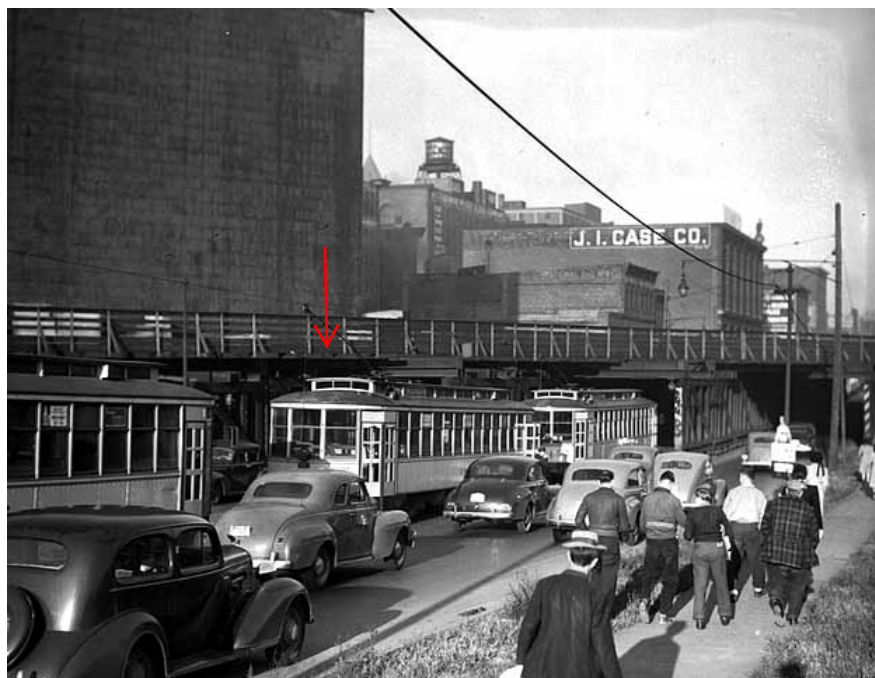




1953 - Minnesota Reflections



1942 - MHS



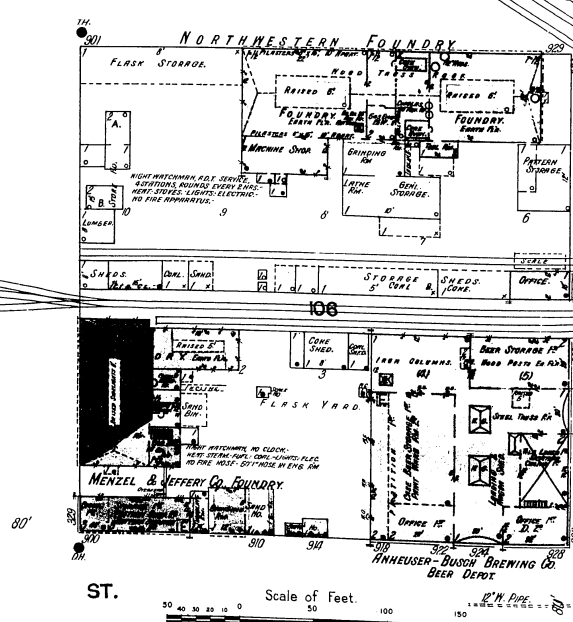
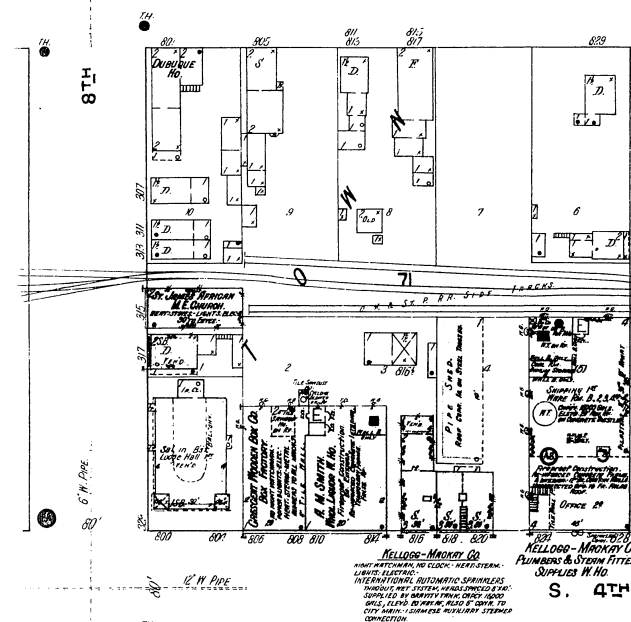
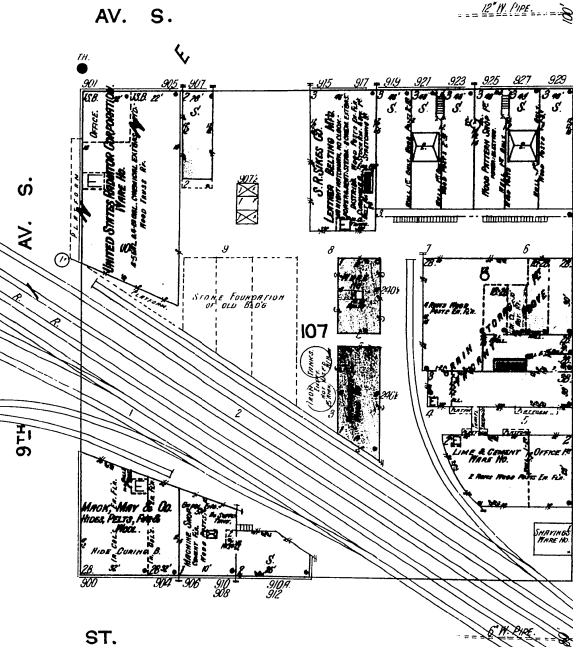
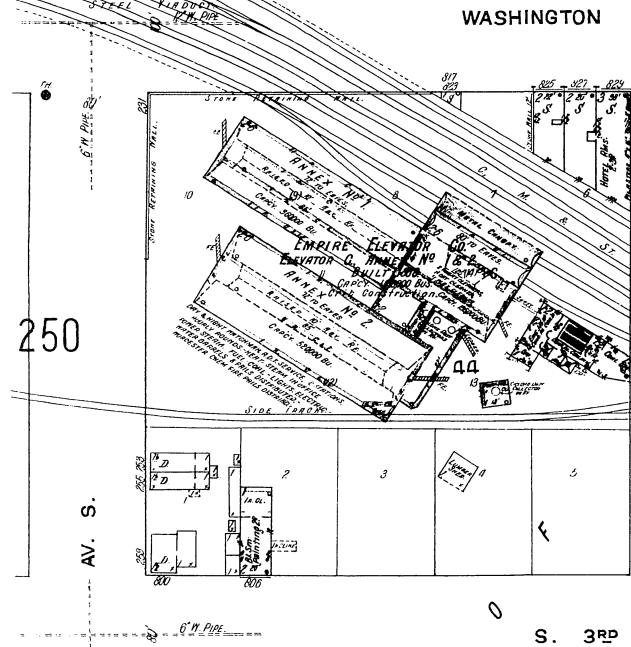
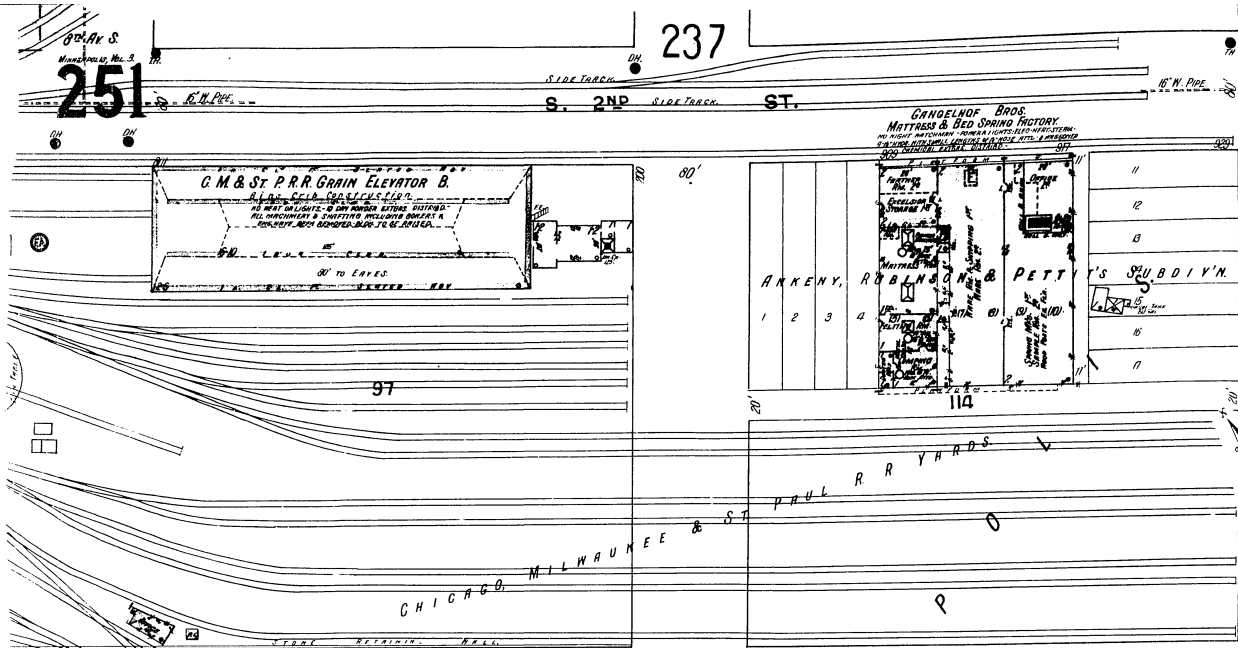
1941 - MHS



1949 - MHS

251

237



264



01.24. 2017

City of Minneapolis

Community Planning & Economic Development

250 S Fourth Street Rm. #300

ATTN: Lisa Steiner / Matthew D. James

RE: IRONCLAD - MPLS

Project Description: The project sits on the property at the southeast corner of Chicago and Washington Avenues in downtown. The site is currently a surface parking lot. The site has approximately 8 feet of grade differential between the high point on the south corner and the low point at the north corner. The proposed development demolishes the parking lot to construct a mixed-use building of 171 market rate residential apartments, 148 hotel units, 8,082 gsf of retail space and 390 structured parking stalls. Phase II of the project consists of 37,632 sf gsf of housing. The hotel resides on the northwest side, the apartments on the northeast side, and the Phase II portion on the southeast side. The first level of the apartment building is designated for retail uses as well as the residential lobby entrance. The southwest side abuts an adjacent property and consists of a two level portion of the parking structure. The project fronts three streets and each street frontage has an active use along the frontage. The hotel lobby, a restaurant, retail space and the residential lobby make up most the length along Washington and Chicago Avenues. Phase II of the project will include residential units above the phase I parking structure along S 9th Avenue. These active uses front a parking structure along all street edges.

The hotel has a twenty foot ground floor lobby and mezzanine with glass and openness to Chicago. This area contains the lobby, lounges, check-in, bar and food service area. The hotel lobby will have direct access to the restaurant on the corner of Chicago and Washington. The hotel is intended to be a Marriot brand hotel with a large pool / rooftop amenity and fitness area that is used jointly with the apartment residents. The restaurant and retail also have a twenty foot first floor height with glass facades and active uses.

The apartments are market-rate units ranging in size from 568 sq. ft. to 1350 sq. ft. with 113 one bedroom units, 58 two bedroom units. The apartments are intended to appeal to a full range of apartment user types. The apartments will have a main entry located on Washington Avenue, and will have dedicated parking access in the lowest level of the parking structure.

The parking structure is two stories above grade and two stories below grade. The lower parking structure is entirely enclosed, with the top of the structure providing a roof deck amenity for the hotel guests as well as the residents. The structure can be entered from Chicago with the main exit at S 9th Avenue. The entrance has an internal drop-off zone for hotel guests, and residents with a secondary

exit along Chicago. Residents / valet have access to the underground level of parking stalls. This level is designated for residents only and has a garage door entrance located off of the drop-off zone. Hotel and commercial parking will be located on the main level, and the upper level of parking. The drop-off zone will be accessible to public users and residents without going through a ticketing area. The residential area will be secured for residents only, and the public parking will have a toll gate at the entrance. Once the public paid area is entered the exit is located at S 9th Avenue. This was established to minimize congestion along Chicago, and to ease overall traffic flow. There is a trash and loading area that can be accessed from S 9th Avenue and is outside of the main traffic zones of Chicago and Washington.

Zoning: The site is zoned B4N and the downtown parking overlay. The site is also identified in the mill district design guidelines as it relates to the setback from Chicago Avenue. The guidelines indicate the setback on Chicago should increase as the site approaches the river. The first floor of the proposed plan has an eight foot setback increasing to twenty feet on the corner of Chicago and Washington.

Building Design: The building design is broken into four parts, the hotel, parking ramp, apartment tower and phase II residential (Phase II will be built above the phase I parking ramp footprint). Each of these parts is intended to screen the parking at the interior of the lot. The locations where these buildings joins there is a distinct building element that provides a transition between the use and height. Although there are four parts, the building is designed to look like one integrated development. The site was used as an industrial elevator with rail access cutting through the site. The northeast portion of the site still indicates the location of the rail corridor. The design highlights this angular gap through the site. At the ground level this rail corridor is expressed as a walkway and does provide access to retail frontage. The building design reflects this historical pattern of the city and utilizes the angle as part of the architectural expression. The first level is designed with very active uses along the street frontage and contains a twenty foot floor height and large amounts of glass. The building façade is pulled back from the property lines. Along Chicago the building is initially setback increases to twenty feet in keeping with the intent of the Chicago Avenue design guidelines that look to provide more openness of the street along Chicago increasing as you approach the river. This setback enhances the hotel entrance, and as it opens up becomes an outdoor terrace for the restaurant. This area does have a noticeable grade change, and the design of the ramp and the entries accommodates the grade change of eight feet to allow grade level access on three sides. The existing site has large limestone blocks as remnants from the railroad retaining wall. The design incorporates the historical wall as the base for the outdoor patio of the restaurant, and some of the remaining blocks will be used as decorative landscape pieces.

Height: The Proposed Building has a range of height and stories. The hotel is 8 stories and 90', the apartments are 14 stories and 147'-6" and the phase II portion is 4 stories above the third-floor plaza and 62'-6" tall at S 9th Avenue. The B4N Zoning allows for 10 stories or 140 feet in height (whichever is less) and can be increased by a conditional use permit per zoning code 546.110. The applicant is requesting a Conditional Use Permit for the additional height.

Floor Area Ratio: The site is 71,863 sf. The proposed building contains 250,831 square feet of use and an F. A. R of 3.50. The allowable F.A.R is unlimited with a minimum of 2.0. The total areas are approximately:

PHASE I

Retail	8,082 sf	
Residential	177,374 sf	171 Units
Hotel	61,876 sf	148 Keys
<u>Plaza Building</u>	<u>3,499 sf*</u>	
	250,831 sf*	
Parking	161,243 sf	

PHASE II (Above Phase I)

Residential 37,632 sf 20 units townhomes (F. A. R of 4.01)

*Note that the residential tower and the hotel share some areas.

*Phase II footprint is built directly over the phase I parking ramp.

Parking: A total of 390 stalls are provided. 232 of the stalls will be dedicated to the residential and valet use. The remaining stalls will be open for use by the hotel (114), retail (20), and restaurant (22). The parking is intended for the uses on the site with a very small percentage available to the public on a first come first serve basis. This would occur on the low-peak times of the other uses. The hotel does contain a food and beverage component that is open to the public but is included in the parking for the hotel. The residential parking will be located on the entire lower level of the footprint for residences and valet only parking for added security. There will also be 171 dedicated residential bike stalls, and an additional 40 bike stalls for the hotel and commercial uses.

Loading: There are two loading areas for the project. Inside the ramp there is a loading zone for the residential apartments. On the east side of the project there is one zone that can accommodate a full size truck. The management of the hotel does not require having full size trucks. The laundry is internal, and even if it was outsourced this is done with smaller vehicles by general practice. Thus a loading zone for the hotel purposes would not be used. This project has consciously been designed to provide an internal drop zone behind the façade of the building instead of a curb cut-out. This area has a circular drive with a 12' ceiling and designed for moderate delivery vehicles at the hotel retail and the residential, and the intent is for the majority of deliveries to happen in this area. Large zone loading areas would not be used.

Setbacks: The building has a 2 foot setback on the south side of the ramp. On Chicago Avenue the first floor building setback starts at ten feet and increases as it approaches the river from 20 feet at the first floor of the restaurant. The upper floor setback is at 8 feet. The building design also provides four feet of additional setback along the Washington retail and opens up from 17 to 20+ feet of setback into the walkway along the old rail corridor opening the possibility for outdoor use along the sidewalk, and

along the walkway. The elevated residential portion along the walkway is set back 4 feet. The building is set on the property line along S 9th Avenue to align with the existing built form.

1. Conditional use permit to increase maximum height

ALL CONDITIONAL USE PERMIT STATEMENT:

(1) The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort, or general welfare.

- The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort, or general welfare. The additional height will not cause any change or difference in the operation or maintenance or safety as there is no difference in use or maintenance with or without the CUP. The additional height does not change any of the conditions in the public realm that would impact general welfare. It should be noted that the building has a range of heights, and although one portion requires a conditional use permit, the other portions are well below the allowable height. This approach allows for more height diversity on the block and is less impactful than a continuous building of the same height.

(2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

- The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. The overall development helps define the public right of way, and improves 9th Avenue as a benefit to neighboring properties. The additional height does not impact any other additional development in the area, nor would it be a detriment.

(3) Adequate utilities, access roads, drainage, necessary facilities, or other measures, have been or will be provided.

- Adequate utilities, access roads, drainage, necessary facilities have been or will be provided. The additional height does not change or increase any of the access conditions, or necessary facilities.

(4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets.

- The parking is intended for the uses on the site with a very small percentage available to the public on a first come first serve basis. The parking is designed to drop off from Chicago Avenue and exit onto S 9th Avenue. Upon event days and high flow exiting the ramp can exit out of both

Chicago Avenue and S 9th Avenue. The additional height has not and would not increase the number of parking stalls or vehicles. The parking is designed by the level, and the reduction of some apartment units would not change the total number of possible vehicles in the facility.

(5) The conditional use is consistent with the applicable policies of the comprehensive plan.

- The conditional use is consistent with the applicable policies of the comprehensive plan.

(6) The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.

- The conditional use is consistent with the other applicable regulations and has neighborhood support. DMNA – 05.03.2016 (Public Meeting) 05.16.2016 (Neighborhood letter of support)

INCREASING MAXIMUM HEIGHT: In addition, the following findings must be addressed if applying for a conditional use permit for increasing maximum height:

(1) Access to light and air of surrounding properties.

- There is little impact to the surrounding properties and their access to light and air.
(See the sun studies diagram)

(2) Shadowing of residential properties, significant public spaces, or existing solar energy systems.

- There is little impact in shadowing the surrounding properties during most the year.
(See the sun studies diagram)

(3) The scale and character of surrounding uses.

- The Proposed Building has a range of height and stories. The hotel is 8 stories, the apartments are 14 stories and the phase II portion is 4 stories above the third-floor plaza deck, all reinforcing the existing fabric along S 9th Avenue, responding to the future scale and setbacks along Chicago Avenue while highlighting the historical industrial scale that once was on the site and how it fronts Washington Avenue.

(4) Preservation of views of landmark buildings, significant open spaces, or water bodies.

- Although there are four parts, the building is designed to look like one integrated development. The site was used as an industrial elevator with rail access cutting through the site. The northeast portion of the site still indicates the location of the rail corridor. The design highlights this angular gap through the site. At the ground level this rail corridor is expressed as a walkway and does provide access to retail frontage. The building design reflects this historical pattern of the city and utilizes the angle as part of the architectural expression. The first level is designed with very active uses along the street frontage and contains a twenty-foot floor height and large amounts of glass. The building façade is pulled back from the property lines. Along Chicago the building is initially setback increases to twenty feet in keeping with the intent of the Chicago Avenue design guidelines that look to provide more openness of the street along Chicago increasing as you approach the river. This setback enhances the hotel entrance, and

as it opens becomes an outdoor terrace for the restaurant. This area does have a noticeable grade change. The design incorporates the historical wall as the base for the outdoor patio of the restaurant. Although it is not in the adjacent Historic District, the height increase respects the Gold Medal Buildings overall height and is well below that height.

1. Variance for northeast interior side yard for residential windows on floors 3-10 that are set back 4 ft from NE lot line.

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

- The site was used as an industrial elevator with rail access cutting through the site. The northeast portion of the site still indicates the location of the rail corridor. The design highlights this angular gap through the site. At the ground level this historical rail corridor is expressed as a partially covered walkway and does provide improved access to retail frontage. The building design reflects this historical pattern of the city and utilizes the angle as part of the architectural expression consistent with the downtown districts mixed use agenda. The property north of the northeast lot line is currently utilized as a parking lot and will have future building restrictions given the sites shape.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

- The property use in this location is first to provide a building massing that responds to the existing trapezoidal shape of the site and second to reconnect Washington Avenue and 9th Avenue via an enhanced pedestrian zone that will activate an otherwise underutilized portion of the site. The team has worked with city staff and this design reflects the importance of this corridor.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

- If granted the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The property north of the northeast lot line is currently utilized as a parking lot and will have future building restrictions given the sites shape. The proposed variance is allowing for minimal amounts of vision glass in the residential portion of the building (floors 3-10). This residential portion of the building is 3 floors above the immediate public realm and the glass that is proposed is there to help break up the façade for added interest. The aim is to create a walkway that is wide enough to be a safe and inviting connection that is reflective of the sites history and capitalizes on its peculiar shape.

Please let me know if any of the items need additional information or clarifications.

Sincerely,
COLLAGE ARCHITECTS

A handwritten signature in black ink, appearing to read 'Pete Keely', with a stylized, cursive script.

Pete Keely, A.I.A.
President





SITE BOUNDARIES



1938



1942 - EAST DOWN WASHINGTON AVE



SITE TODAY - SOUTHEAST CORNER OF WASHINGTON & CHICAGO AVENUES



IRONCLAD

LIVE. WORK. EXPLORE.

MPLS, MN

01.12.2017

Throughout Minneapolis' history, this site was a substantial contributor to the area's involvement in the National grain exchange. Constructed in 1867, The Union Grain Elevator (or "Elevator E") that once stood on the site was one of the earliest and largest wood grain elevators built in Minneapolis (able to hold up to 130,000 bushels of grain). The elevator was located directly on the Chicago, Milwaukee, and Saint Paul railway that ran towards the adjacent Mill District along the Mississippi.

Before grain elevators were constructed in concrete, wood elevators of this kind were often prone to fire. After just 10 years of use, The Union elevator was burned to the ground, reconstructed, and IRON CLAD to better protect the structure against future fires.

Today, the only visible remnant of this site's historical importance is the stone wall that supported the railway viaduct spanning Washington Avenue.



Collage | architects

KHARBANDA MANAGEMENT



GRAVES HOSPITALITY

DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.



SOUTHEAST VIEW FROM CORNER OF WASHINGTON & CHICAGO AVENUES



SOUTHWEST VIEW FROM WASHINGTON AVENUE



NORTHEAST VIEW FROM CHICAGO AVENUE



NORTHWEST VIEW FROM S 9TH AVENUE





SITE PLAN: NTS



IRONCLAD

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MPLS, MN

01.12.2017



AXONOMETRIC



Collage | architects

KHARBANDA MANAGEMENT



GRAVES HOSPITALITY
DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.



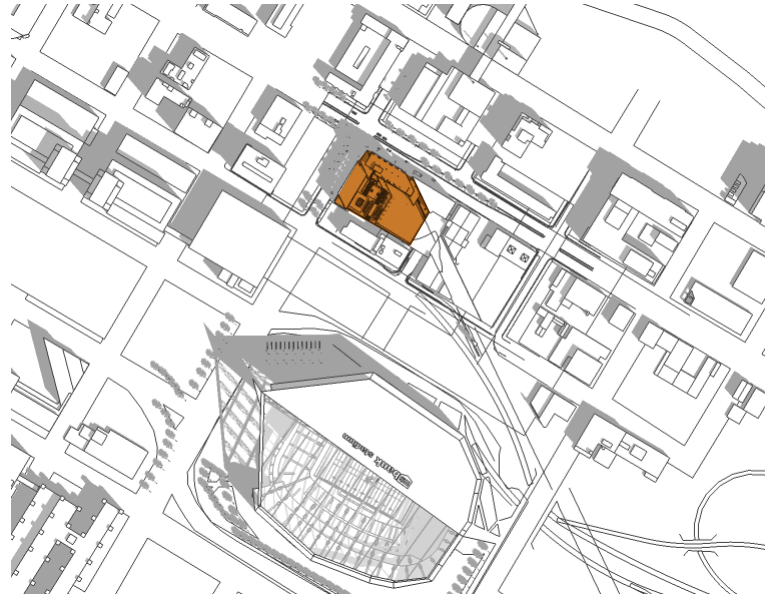
IRONCLAD
LIVE. WORK. EXPLORE.

MPLS, MN
01.12.2017



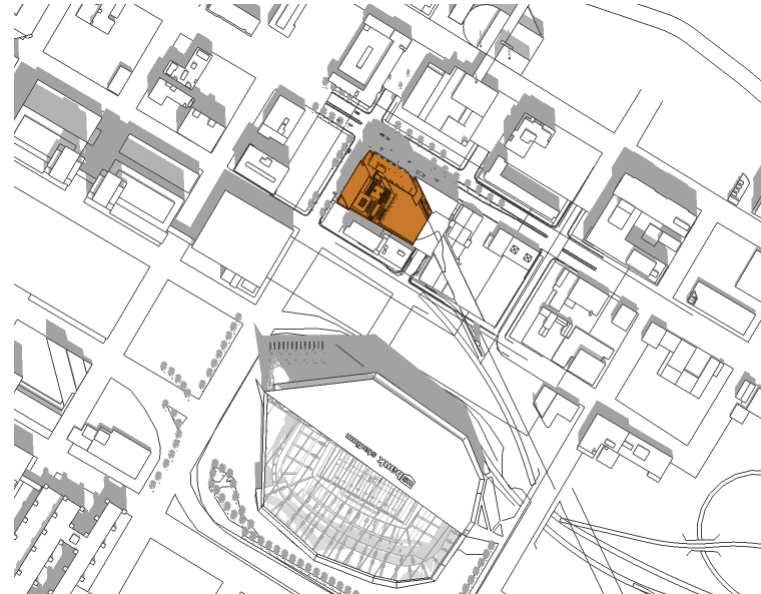
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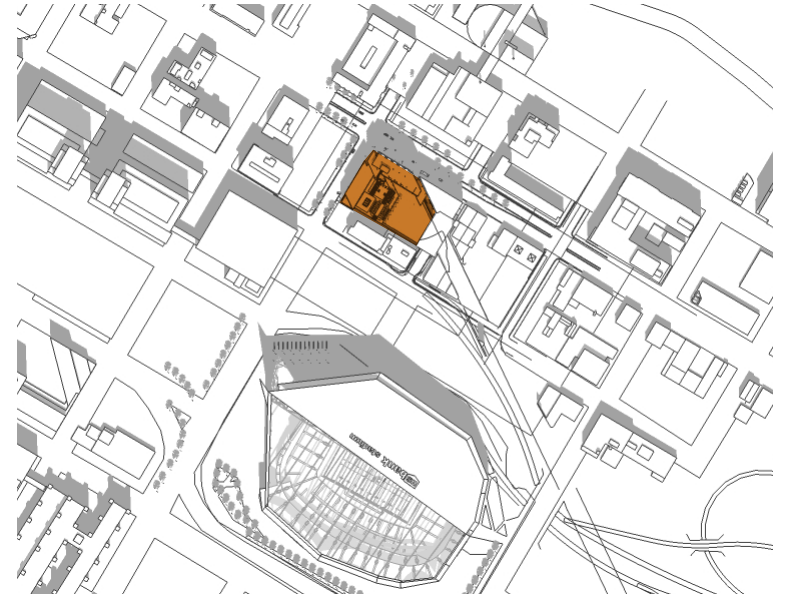
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10:00AM



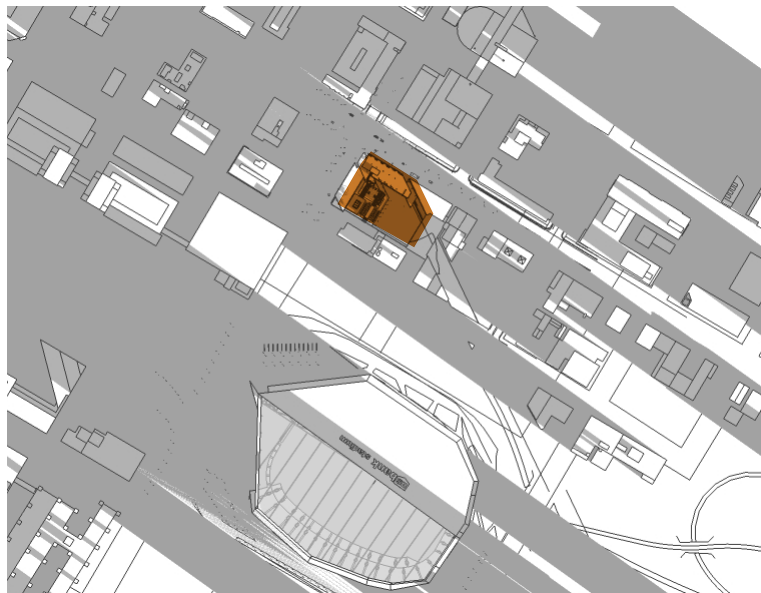
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SEP 21, 2015

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DEC 21, 2015

8:00AM



DEC 21, 2015

10:00AM



DEC 21, 2015

12:00PM



DEC 21, 2015

4:00PM



Collage | architects

KHARBANDA MANAGEMENT



GRAVES HOSPITALITY
DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.

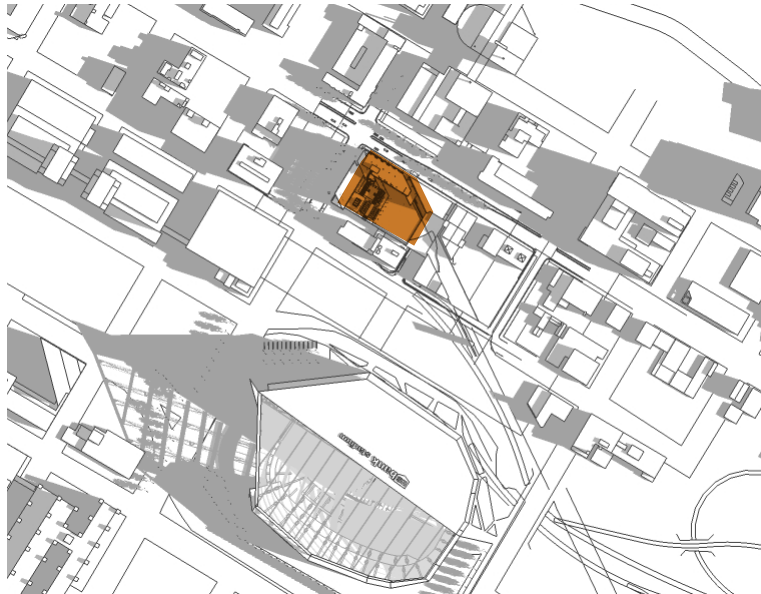


IRONCLAD

LIVE. WORK. EXPLORE.

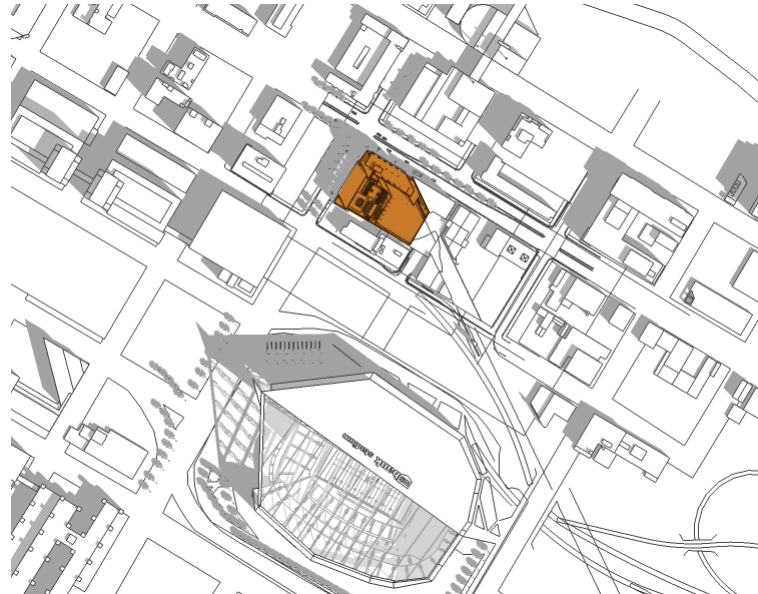
MPLS, MN

01.12.2017



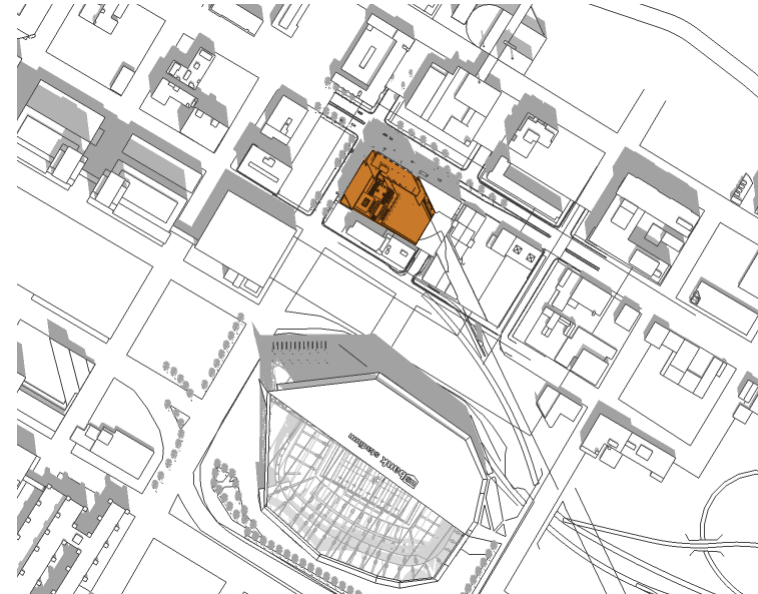
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8:00AM



MAR 21, 2015

10:00AM



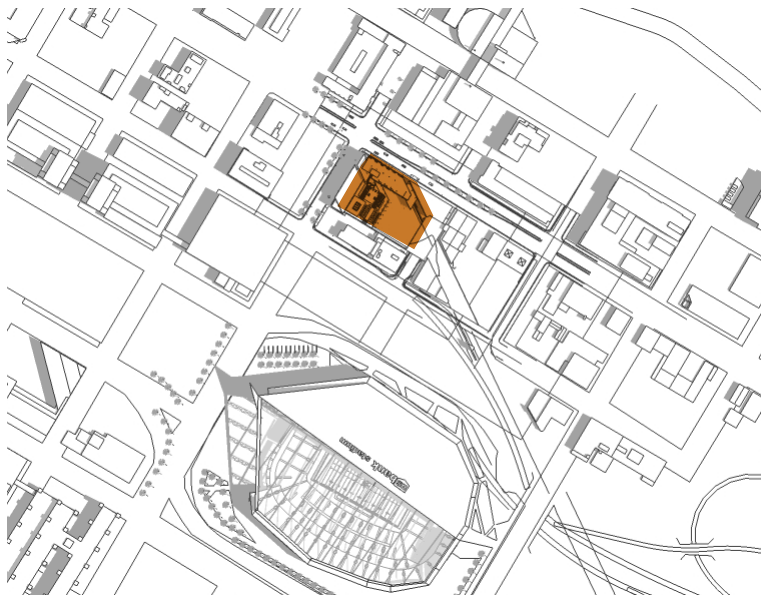
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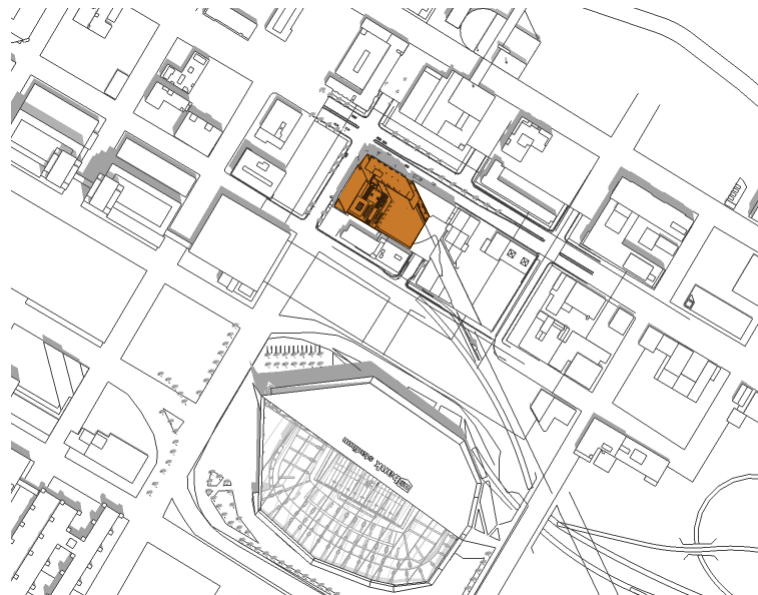
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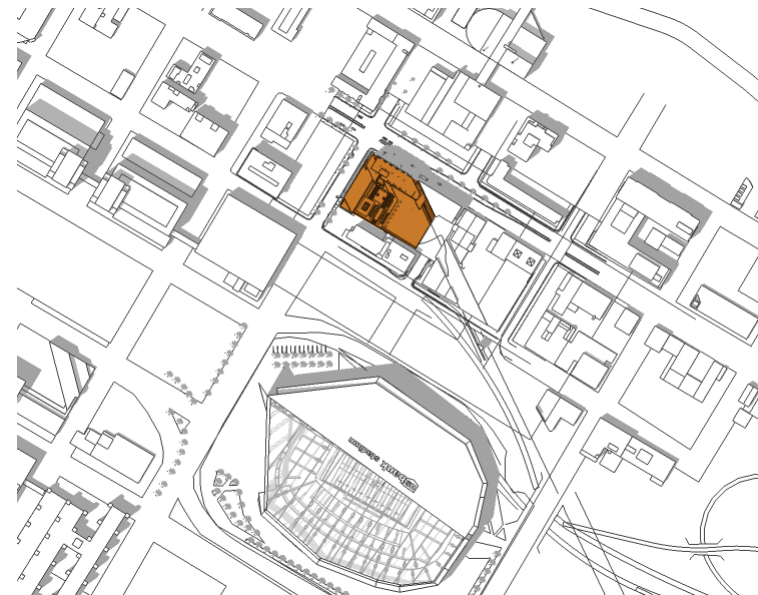
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10:00AM



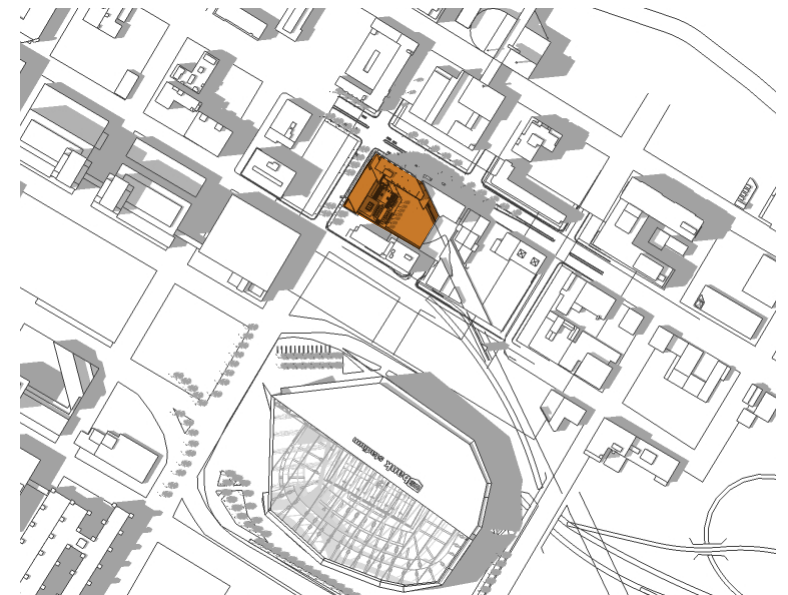
JUN 21, 2015

12:00PM



JUN 21, 2015

2:00PM



JUN 21, 2015

4:00PM



Collage | architects

KHARBANDA MANAGEMENT



GRAVES HOSPITALITY
DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.

MATERIAL INDEX

- 01

BURNISHED BLOCK & SILL
(COLOR: 'MIDNIGHT')
- 02

NORMAN FACE BRICK
(COLOR: BELDEN 'ASHBERRY VELOUR A')
- 03

NORMAN FACE BRICK
(COLOR: BELDEN 'ASHBERRY VELOUR A')
- 04

2' GRANITE BASE
(COLOR: MESABI DIAMOND BLACK 100)
- 05

CAST IN PLACE CONCRETE
(COLOR: NATURAL GRAY)
- 06

PREFIN. METAL PANEL #1
(COLOR: GUNMETAL GREY)
- 06B

6" PREFIN. METAL TRIM #1
(COLOR: GUNMETAL GREY)
- 07

PREFIN. METAL PANEL #2
(COLOR: CHAMPAIGN METALLIC)
- 07B

6" PREFIN. METAL TRIM #2
(COLOR: ANODIZED METALLIC)
- 08

PREFIN. METAL PANEL #3
(COLOR: ANODIZED METALLIC)
- 08B

6" PREFIN. METAL TRIM #3
(COLOR: ANODIZED METALLIC)
- 09

PREFIN. STANDING SEAM METAL PANEL #1
(COLOR: CUSTOM PURPLE)
- 10

PREFIN. ACCENT FLAT METAL PANEL #1
(COLOR: CUSTOM PURPLE)
- 11

PREFIN. ACCENT METAL PANEL #2
(3" BLACKED CORTEN STEEL PANELS)
- 12

PREFIN. ACCENT METAL PANEL #3
(18 GA. BLACKED CORTEN STEEL PANELS)
- 13

BENT MTL GAURDRAIL / PERF MTL INFILL
(COLOR: POWDER COATED BLACK / ANODIZED)
- 14

STAINLESS STL GAURDRAIL / HORZ. CABLE
(COLOR: STAINLESS STEEL)
- 15

STEEL SIGNAGE BAND
(GALVANIZED)
- 16

ILLUMINATED SIGNAGE
(SEPARATE PERMIT IS REQUIRED)
- 17

ALUMINUM STOREFRONT SYSTEM
(COLOR: ANODIZED ALUMINUM)
- 18

VINYL/COMPOSITE WINDOWS
(COLOR: BLACK W/ INTEGRAL GRILLS)
- 19

VINYL/COMPOSITE WINDOWS
(COLOR: SILVER W/ INTEGRAL GRILLS)
- 20

ALUMINUM WINDOW WALL SYSTEM
(COLOR: ANODIZED ALUMINUM)
- 21

ALUMINUM PERFORATED WINDOW SYSTEM
(COLOR: ANODIZED ALUMINUM)
- 22

ALUMINUM THREE PANEL DOOR SYSTEM
(COLOR: ANODIZED ALUMINUM)
- 23

ACCENT LIGHTING
- 24

LED LIGHTING
(COMPLY W/ MOXY STANDARDS OF 2700K WARM)
- 25

MECHANICAL SCREENING
(COMPLY W/ SECTION 535.70 OF THE ZONING CODE)



NORTH ELEVATION 1/32" = 1'-0"



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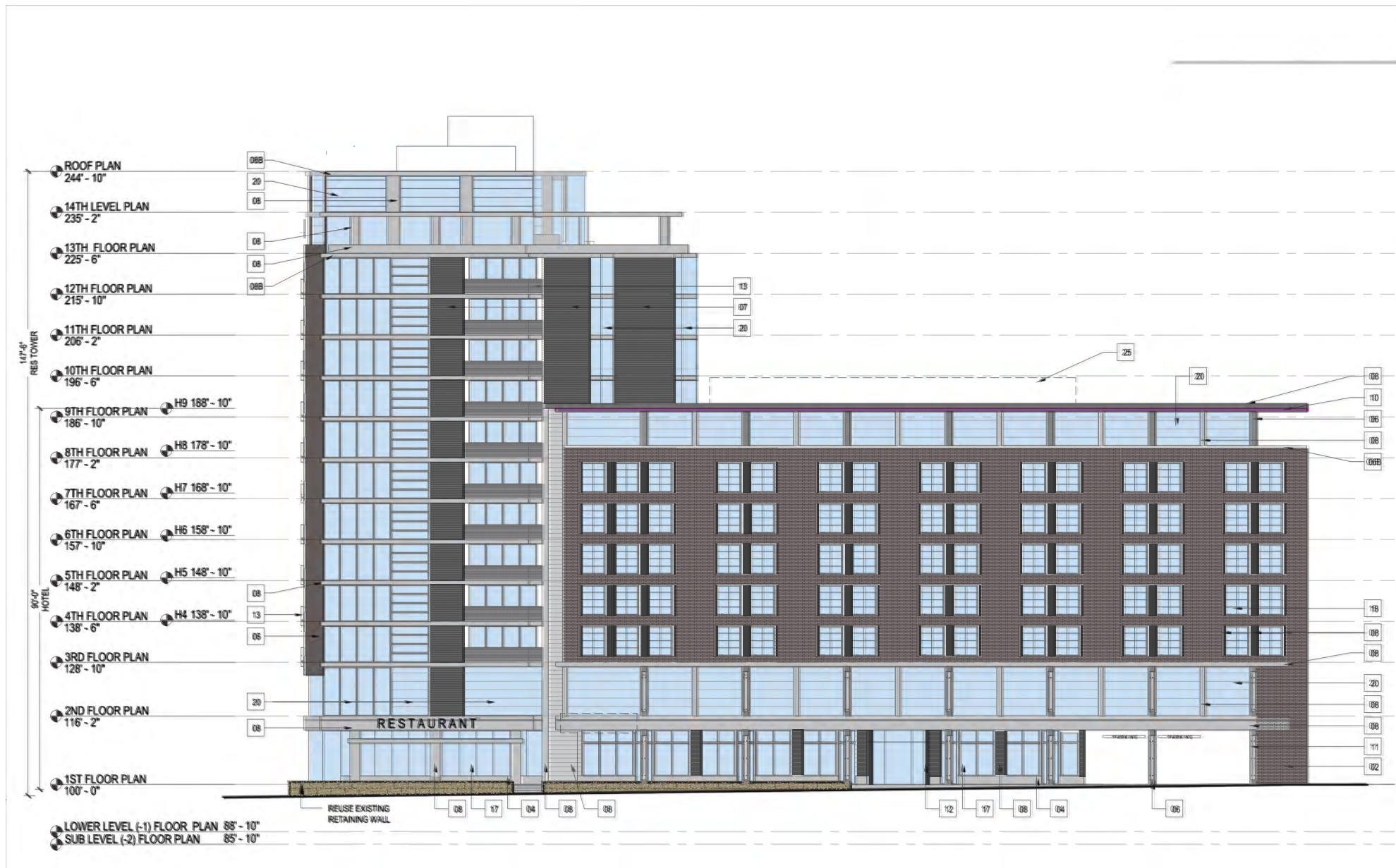
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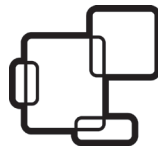
01.12.2017

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11	PREFIN. ACCENT METAL PANEL #2 (1/2" BLACKED CORTEN STEEL PANELS)
12	PREFIN. ACCENT METAL PANEL #3 (1/8 GA. BLACKED CORTEN STEEL PANELS)
13	BENT MTL GAURDRAIL / PERF MTL INFILL (COLOR: POWDER COATED BLACK / ANODIZED)
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WEST ELEVATION 1/32" = 1'-0"



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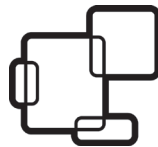
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EAST ELEVATION 1/32" = 1'-0"



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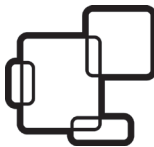
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EAST ELEVATION (PHASE II) 1/32" = 1'-0"

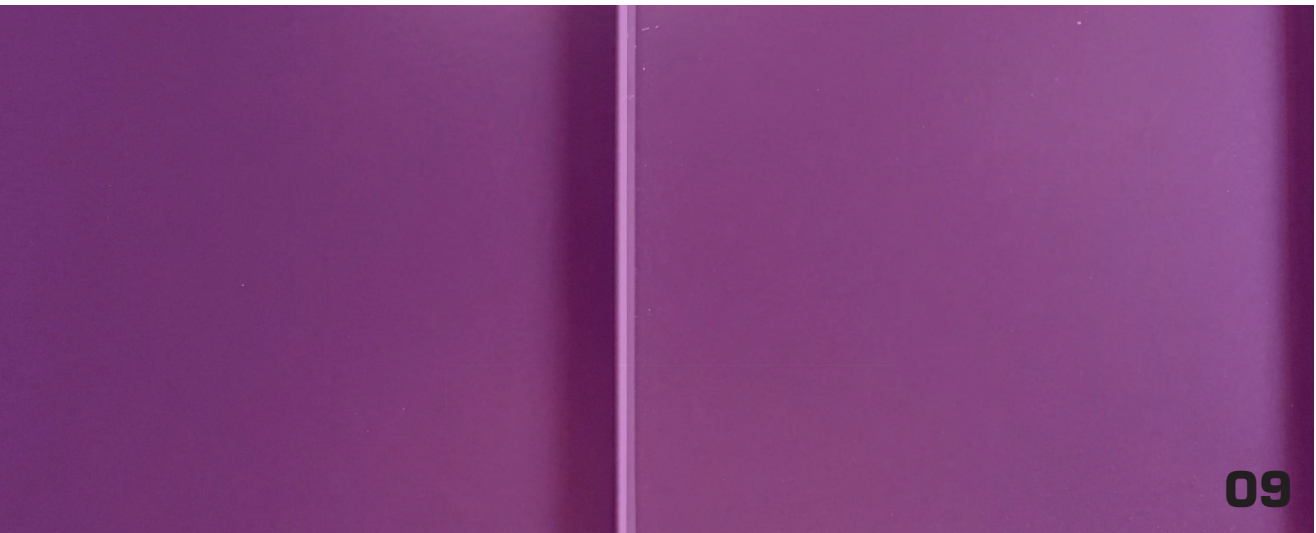


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01.12.2017

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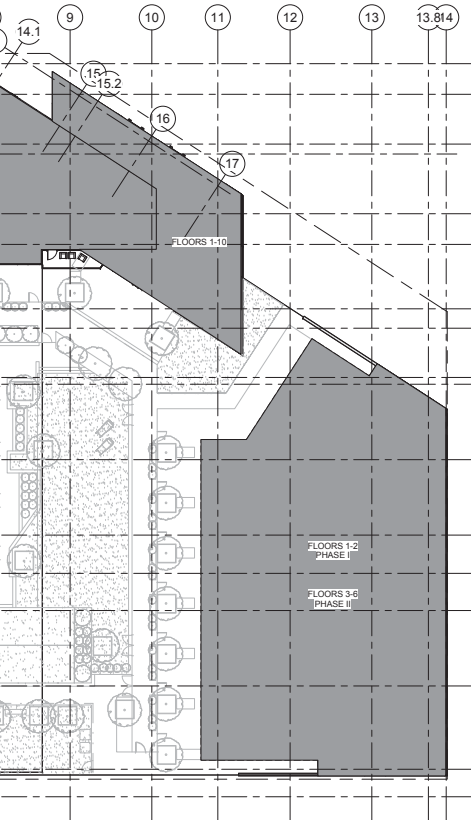
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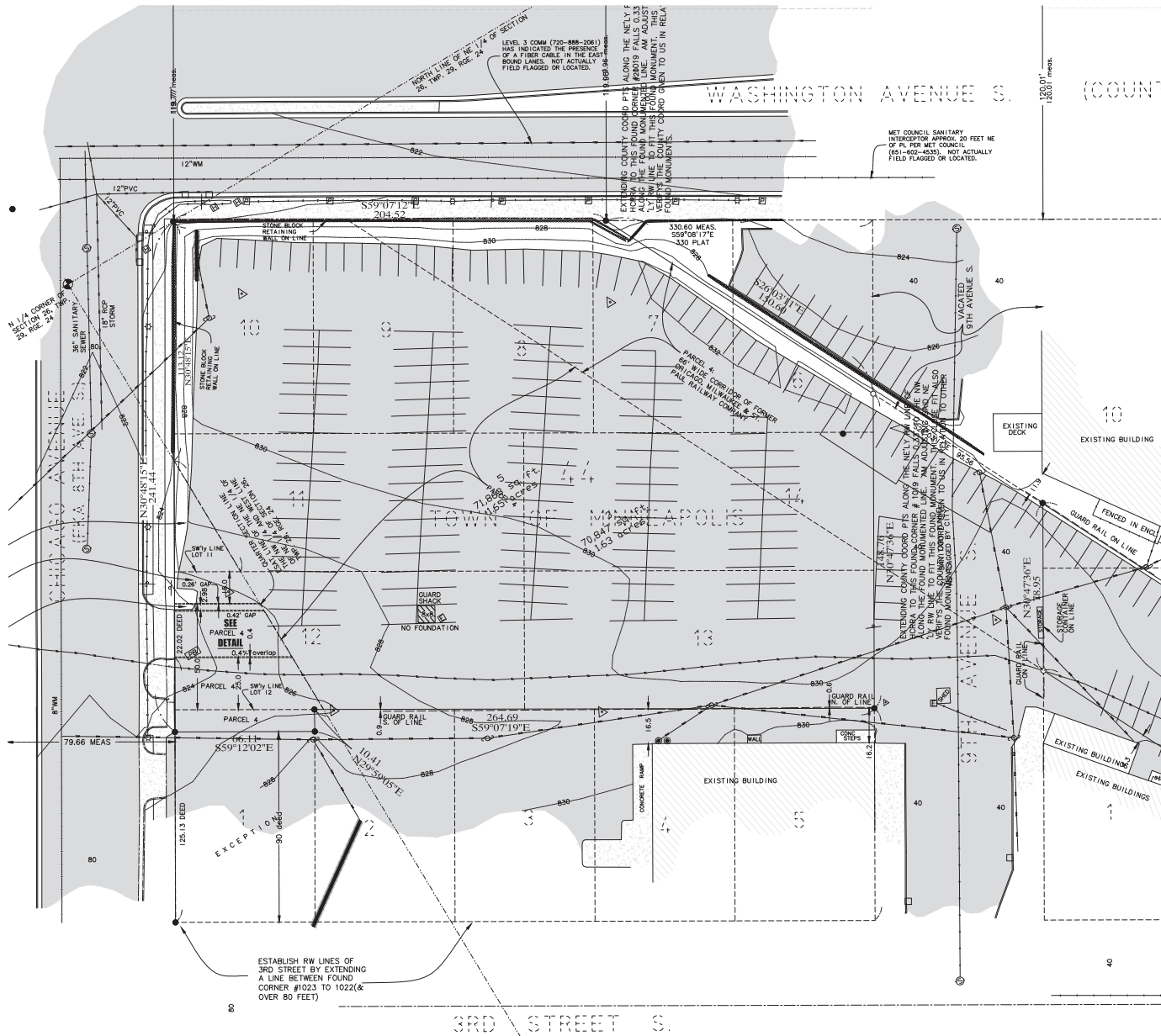
GRAVES HOSPITALITY

DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.

Grand total: 392

AREA SCHEDULE (GROSS BUILDING)			
Level	Area	Keys	Units
SUB LEVEL (-2) FLOOR PLAN	11,057 SF		
LOWER LEVEL (-1) FLOOR PLAN	64,235 SF		
1ST FLOOR PLAN	63,058 SF		
2ND FLOOR PLAN	51,514 SF		
3RD FLOOR PLAN	51,514 SF	25	13
4TH FLOOR PLAN	23,216 SF	25	16
5TH FLOOR PLAN	23,216 SF	25	16
6TH FLOOR PLAN	23,216 SF	25	16
7TH FLOOR PLAN	23,216 SF	25	16
8TH FLOOR PLAN	22,921 SF	23	16
9TH FLOOR PLAN	16,000 SF		
10TH FLOOR PLAN	15,000 SF		
11TH FLOOR PLAN	13,168 SF		
12TH FLOOR PLAN	13,430 SF		
13TH FLOOR PLAN	11,570 SF		9
14TH LEVEL PLAN	11,246 SF		
	1,420.28 SF	148	171

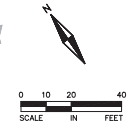




LEGEND

- DENOTES IRON MONUMENT FOUND AS LABELED
- DENOTES MONUMENT SET (AS LABELED)
- ⊙ DENOTES HENNEPIN COUNTY MONUMENT
- ⊙ DENOTES BOLLARD
- ⊙ DENOTES PARKING METER
- ⊙ DENOTES PARKING PAY BOX
- ⊙ DENOTES STORM SEWER CATCH BASIN
- ⊙ DENOTES STORM SEWER MANHOLE
- ⊙ DENOTES SANITARY SEWER MANHOLE
- ⊙ DENOTES TELEPHONE MANHOLE
- ⊙ DENOTES WATER MANHOLE
- ⊙ DENOTES MISCELLANEOUS MANHOLE
- ⊙ DENOTES HYDRANT
- ⊙ DENOTES GAS METER
- ⊙ DENOTES POWER POLE
- ⊙ DENOTES EXISTING SPOT ELEVATION
- ⊙ DENOTES SIGN
- ⊙ DENOTES LIGHT POLE
- ⊙ DENOTES BILLBOARD SIGNAGE
- ⊙ DENOTES HANDHOLE
- ⊙ DENOTES ELECTRICAL BOX
- ⊙ DENOTES EXISTING 2 FOOT CONTOURS
- ⊙ DENOTES GUARD RAIL
- ⊙ DENOTES RETAINING WALL
- ⊙ DENOTES EXISTING SANITARY SEWER
- ⊙ DENOTES EXISTING STORM SEWER
- ⊙ DENOTES EXISTING WATER MAIN
- ⊙ DENOTES OVERHEAD WIRE
- ⊙ DENOTES UNDERGROUND ELECTRIC LINE
- ⊙ DENOTES UNDERGROUND CABLE LINE
- ⊙ DENOTES UNDERGROUND TELE/COMMUNICATION LINE
- ⊙ DENOTES UNDERGROUND FIBER OPTIC LINE
- ⊙ DENOTES CONCRETE SURFACE
- ⊙ DENOTES BITUMINOUS SURFACE

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IRONCLAD
 811 WASHINGTON AVENUE
 MINNEAPOLIS, MN



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 Architect
 Peter Kaszy
 451-472-0050
 705 Raymond Avenue #200
 St. Paul, Minnesota 55114
 Alliant Engineering, Inc.
 Civil Engineer
 Mark Kronbeck
 612-767-9338
 b.e. landscape designs
 LANDSCAPE ARCHITECT
 BEN ERICKSON
 612-382-0902

DATE: 1/12/17

PDR/LUA RESUBMIT

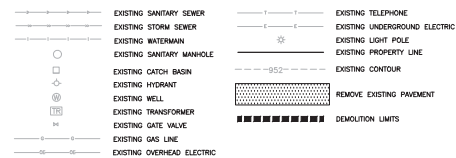
NO.	DATE	DESCRIPTION
1	1/12/17	PDR/LUA RESUBMIT
2		
3		
4		
5		
6		
7		
8		
9		
10		

PROJECT NO: 215-0154
 DRAWN BY: CMK
 CHECKED BY: CMK

SHEET NO: 1

EXISTING CONDITIONS

C-1.0

[illegible]

DATE: _____

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED CIVIL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

CLARK WICKLUND, P.E.
REGISTRATION NO: 26222

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Architect
Pete Keely
651.472.0050
705 Raymond Avenue #200

Alliant Engineering, Inc.
Civil Engineer
Mark Kronbeck
612.767.9338

b.e. landscape designs
LANDSCAPE ARCHITECT
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DATE:	1/12/17
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PDR/LUA RESUBMIT

WORK	DATE	DESCRIPTION
	8-8-16	PDR SUBMISSION
	1-12-17	PDR/LIA REQUEST

PROJECT NO:	215-0154
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10

SHEET TITLE

PRELIMINARY DEMOLITION PLAN

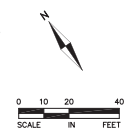
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PARKING DATA				
PARKING REQUIRED				
TYPE	SQUARE FEET	UNITS	MAXIMUM PARKING ALLOWED	MAXIMUM PARKING ALLOWED
Retail	5,130	NA	1 space per 500sf of GFA except that the maximum parking requirement for grocery stores shall be 1 space per 300 sf	10
Restaurant & Patis	2,832	NA	1 space per 200sf of GFA	14
Hotel	NA	150	82% of the capacity of persons except that the maximum requirement for hotels shall be 1 space per guestroom + parking equals 30% capacity of persons for affiliated uses such as dining or meeting rooms	150
Apartments	NA	169	1.5 spaces per dwelling in the 68th district. Visitor parking at least 1 space per 50 dwellings/units. ADA stalls per Min State Bldg Code.	270
Total				465
				streets

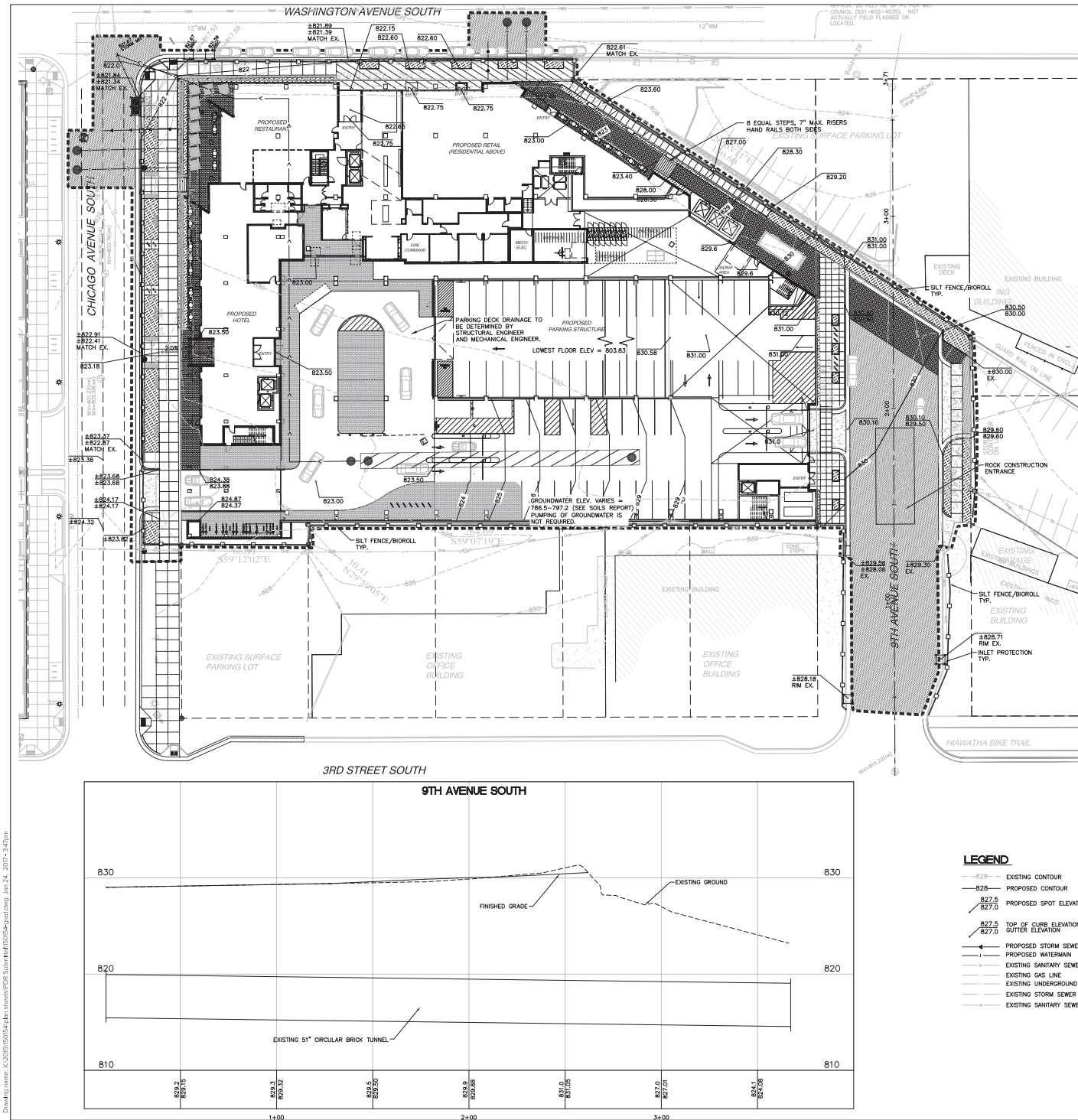
PARKING PROVIDED	
Underground & Above Ground Parking	390
Total	390
	staff

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GRADING NOTES:

- ALL PAVEMENTS SHALL SLOPE AWAY FROM EXISTING AND PROPOSED BUILDINGS.
- THE CONTRACTOR SHALL KEEP THE ADJACENT ROADWAYS FREE OF DEBRIS AND PREVENT THE OFF-SITE TRACKING OF SOIL IN ACCORDANCE WITH THE REQUIREMENTS OF THE CITY/COUNTY.
- NOTIFY Gopher State One Call, AT (800)252-1166, 48 HOURS PRIOR TO START OF CONSTRUCTION.
- ALL IMPROVEMENTS TO CONFORM WITH CITY AND COUNTY CONSTRUCTION STANDARDS SPECIFICATION, LATEST EDITION.
- 50' OF BITUMINOUS OR ROCK CONSTRUCTION ENTRANCES SHALL BE PROVIDED AT ALL CONSTRUCTION ACCESS POINTS.
- CONTRACTOR TO KEEP A COPY OF THE FINAL DRAINAGE REPORT ON SITE AT ALL TIMES.
- SURVEYOR TO VERIFY BENCHMARK ELEVATION PRIOR TO START OF CONSTRUCTION.
- REFER TO GEOTECHNICAL REPORT AND PROJECT MANUAL, FOR SOIL CORRECTION REQUIREMENTS AND TESTING REQUIREMENTS.
- STOP TOPSOIL PRIOR TO ANY CONSTRUCTION. REUSE STOCKPILE ON SITE.
- REFER TO SITE DESIGN CRITERIA SECTION OF PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS.
- PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE TO MAKE SURE THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PLANS AND OTHER DOCUMENTS APPROVED BY ALL OF THE PERMITTING AGENCIES.

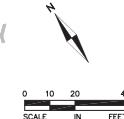
EROSION PREVENTION AND SEDIMENT CONTROL NOTES:

- CONTRACTOR MUST NOTIFY CITY OF MINNEAPOLIS ENGINEERING AND DESIGN (612-673-2738) 48 HOURS PRIOR TO ANY LAND DISTURBANCES. FAILURE TO DO SO MAY RESULT IN THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.
- Install perimeter erosion control at the locations shown on the plans prior to beginning construction. (Hay Bales are not an acceptable perimeter control)
- Before beginning construction, install a TEMPORARY ROCK CONSTRUCTION ENTRANCE at each point where vehicles exit the construction site. Use 3 inch or greater diameter rock in a layer of at least 6 inches thick across the entire width of the entrance. Extend the rock entrance at least 50 feet into the construction zone. Use a geotextile fabric beneath the aggregate in order to prevent migration of soil into the rock from below.
- Remove all soils and sediments tracked or otherwise deposited onto public and private pavement areas. Removal shall be on a daily basis when tracking occurs. Sealing may be ordered by at any time if conditions warrant. Sealing shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
- Install inlet protection at all public and private catch basin inlets, which receive runoff from the disturbed areas. Catch basin inserts are required in undisturbed areas. Staged silt fence or other approved BMP's in disturbed areas. NOTE: HAY BALES OR FILTER FABRIC UNDER THE GRATES ARE NOT EFFECTIVE OR AN ACCEPTABLE FORM OF INLET PROTECTION.
- Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tarps, or other means. Control erosion from all stockpiles by placing silt fence barriers around the piles. Temporary stockpiles located on paved surface must be no less than two feet from the drainage/gutter line and shall be covered if left more than 24 hours.
- Maintain all temporary erosion and sediment control devices in place until the contributing drainage area has been stabilized. Inspect temporary erosion and sediment control devices on a daily basis and replace deteriorated, damaged, or rotted erosion control devices immediately.
- Temporarily or permanently stabilize all denuded areas which have been final-graded, and all denuded areas in which grading or site building construction operations are not actively underway against erosion due to rain, wind and running water within 14 days. Use seeding and mulching, erosion control matting, and/or sodding and staking in green space areas. Use early application of gravel base on areas to be paved.
- Remove all temporary synthetic, structural, non-biodegradable erosion and sediment control devices after the site has undergone final stabilization and permanent vegetation has been established, minimum vegetation establishment is 70% cover, maintain all temporary erosion control devices until 70% established cover is achieved.
- Ready mixed concrete and concrete batch plants prohibited within the public right of way, designate concrete mixing/washout locations in the erosion control plan. Under no circumstances may washout water drain onto the public right of way or into the public storm sewer.

AREA OF DISTURBANCE

DISTURBANCE AREA = 100,187 SF, 2.30 ACRES
EXISTING IMPERVIOUS = 1.94 ACRES
(WITHIN DISTURBANCE AREA)
PROPOSED IMPERVIOUS = 2.05 ACRES
(WITHIN DISTURBANCE AREA)

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PRELIMINARY
NOT FOR CONSTRUCTION



IRONCLAD

811 WASHINGTON AVENUE
MINNEAPOLIS, MN

KHARBANDA MANAGEMENT



DATE:

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LANDSCAPE ARCHITECT
BEN ERICKSON
612-382-0902

DATE: 1/12/17

PDR/LUA RESUBMIT

DATE	BY	REVISION
1-13-17	PK	PRELIMINARY
1-24-17	PK	PRELIMINARY

PROJECT: 215-0154

OWNER: CM

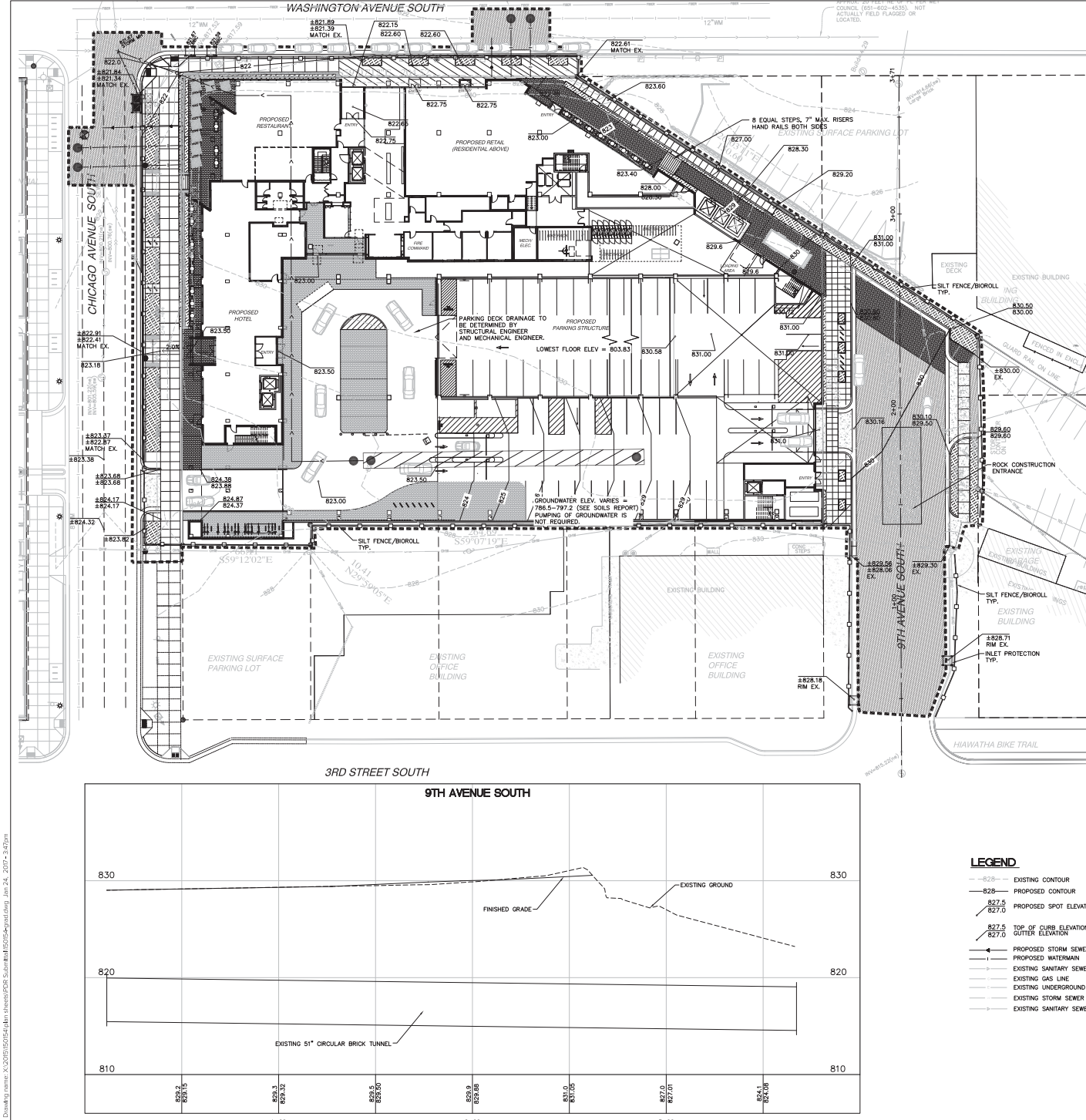
DESIGN: CM

SHEET:

PRELIMINARY GRADING, DRAINAGE & EROSION CONTROL PLAN

C-4.0

Drawing name: X:\2018\152451.dgn sheets\PCS 5.dwg Date: 1/25/2017 1:25:00 PM User: jason.dwyer



GRADING NOTES:

1. ALL PAVEMENTS SHALL SLOPE AWAY FROM EXISTING AND PROPOSED BUILDINGS.
2. THE CONTRACTOR SHALL KEEP THE ADJACENT ROADWAYS FREE OF DEBRIS AND PREVENT THE OFF-SITE TRACKING OF SOIL IN ACCORDANCE WITH THE REQUIREMENTS OF THE CITY/COUNTY.
3. NOTIFY Gopher State One Call, AT (800)252-1166, 48 HOURS PRIOR TO START OF CONSTRUCTION.
4. ALL IMPROVEMENTS TO CONFORM WITH CITY AND COUNTY CONSTRUCTION STANDARDS SPECIFICATION, LATEST EDITION.
5. 50' OF BITUMINOUS OR ROCK CONSTRUCTION ENTRANCES SHALL BE PROVIDED AT ALL CONSTRUCTION ACCESS POINTS.
6. CONTRACTOR TO KEEP A COPY OF THE FINAL DRAINAGE REPORT ON SITE AT ALL TIMES.
7. SURVEYOR TO VERIFY BENCHMARK ELEVATION PRIOR TO START OF CONSTRUCTION.
8. REFER TO GEOTECHNICAL REPORT AND PROJECT MANUAL, FOR SOIL CORRECTION REQUIREMENTS AND TESTING REQUIREMENTS.
9. STOP TOPSOIL PRIOR TO ANY CONSTRUCTION. REUSE STOCKPILE ON SITE.
10. REFER TO SITE DESIGN CRITERIA SECTION OF PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS.
11. PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE TO MAKE SURE THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PLANS AND OTHER DOCUMENTS APPROVED BY ALL OF THE PERMITTING AGENCIES.

EROSION PREVENTION AND SEDIMENT CONTROL NOTES:

- 1) CONTRACTOR MUST NOTIFY CITY OF MINNEAPOLIS ENGINEERING AND DESIGN (612-673-2738) 48 HOURS PRIOR TO ANY LAND DISTURBANCES. FAILURE TO DO SO MAY RESULT IN THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.
- 2) Install perimeter erosion control at the locations shown on the plans prior to beginning construction. (Hay Bales are not an acceptable perimeter control)
- 3) Before beginning construction, install a TEMPORARY ROCK CONSTRUCTION ENTRANCE at each point where vehicles exit the construction site. Use 3 inch or greater diameter rock in a layer of at least 6 inches thick across the entire width of the entrance. Extend the rock entrance at least 50 feet into the construction zone. Use a geotextile fabric beneath the aggregate in order to prevent migration of soil into the rock from below.
- 4) Remove all soils and sediments tracked or otherwise deposited onto public and private pavement areas. Removal shall be on a daily basis when tracking occurs. Sealing may be ordered by at any time if conditions warrant. Sealing shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
- 5) Install inlet protection at all public and private catch basin inlets, which receive runoff from the disturbed areas. Catch basin inserts are required in undisturbed areas. Sloped silt fence or other approved BMP's in disturbed areas. NOTE: HAY BALES OR FILTER FABRIC UNDER THE GRATES ARE NOT EFFECTIVE OR AN ACCEPTABLE FORM OF INLET PROTECTION.
- 6) Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tarps, or other means. Control erosion from all stockpiles by placing silt fence barriers around the piles. Temporary stockpiles located on paved surface must be no less than two feet from the drainage/gutter line and shall be covered if left more than 24 hours.
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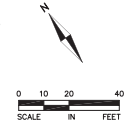
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(WITHIN DISTURBANCE AREA)

LEGEND

- 826- EXISTING CONTOUR
- 826- PROPOSED CONTOUR
- 827.5 PROPOSED SPOT ELEVATION
- 827.5 TOP OF CURB ELEVATION
- 827.5 GUTTER ELEVATION
- PROPOSED STORM SEWER
- PROPOSED WATERMAN
- EXISTING SANITARY SEWER
- EXISTING GAS LINE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING STORM SEWER
- EXISTING SANITARY SEWER
- 826- DIRECTION OF DRAINAGE
- F.F.E. FIRST FLOOR ELEVATION
- L.G.E. LOWEST GARAGE FLOOR ELEVATION
- BIOROLL/SILT FENCE
- INLET PROTECTION
- PROPERTY LINE
- LIMITS OF CONSTRUCTION
- ROCK CONSTRUCTION ENTRANCE

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BEN ERICKSON
612-382-0902

DATE: _____ 1/21/17

PDR/LUA RESUBMIT

DATE	BY	REVISION
1-23-17	PK	PRELIMINARY
1-24-17	PK	PRELIMINARY

PROJECT: 215-0154
OWNER: CMK
DESIGN: CM

SHEET: _____

PRELIMINARY GRADING, DRAINAGE & EROSION CONTROL PLAN

C-4.0



IRONCLAD

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MINNEAPOLIS, MN

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Minneapolis, MN 55415
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612.758.3099 FAX
www.alliant-inc.com

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DATE:	1/12/17
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PDR/LUA RESUBMIT

DATE	DATE	DESCRIPTION
	8-8-16	PDR SUBMISSION
	1-12-17	PDR/LUA RESUBMIT

PROJECT NO:	215-0154
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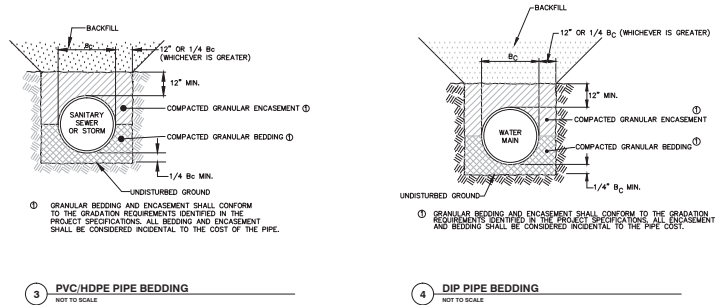
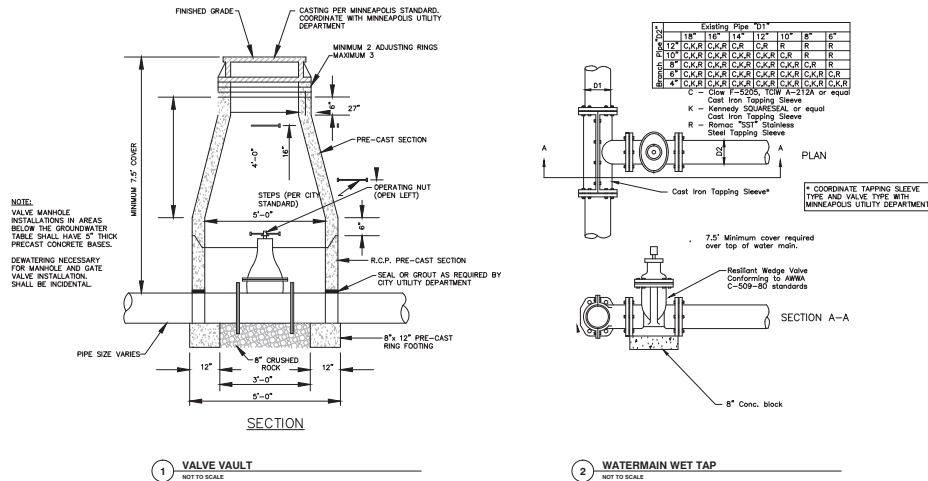
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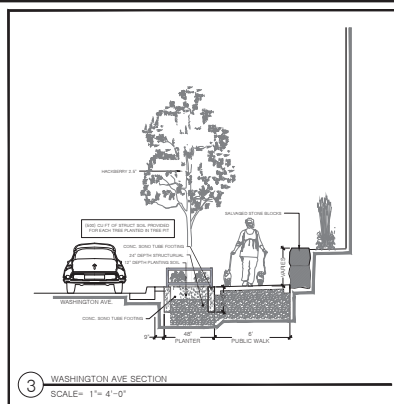
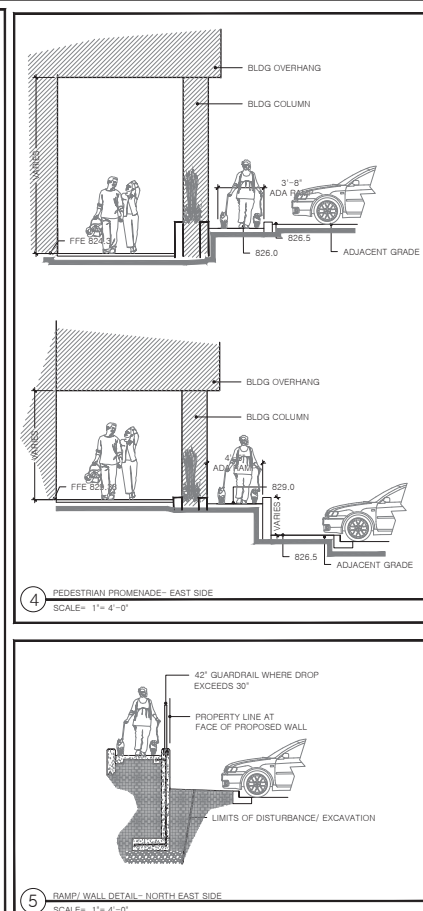
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DETAILS

C-6.0



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6 LANDSCAPE/ SITE NOTES
SCALE= NTS



QTY	COMMON NAME	BOTANICAL NAME	SIZE	CONT	REMARKS
15	Deciduous Canopy Trees				
5	Hackberry	<i>Celtis occidentalis</i>	2.5'	BB/ CONT	12' HGT MIN.
4	Ironwood	<i>Ostrya virginiana</i>	2.5'	BB/ CONT	12' HGT MIN.
6	Kentucky Coffeetree 'Espresso'	<i>Gymnocladus dioica</i> 'Espresso'	2.5'	BB/ CONT	12' HGT MIN.
3	Ornamental Trees				
4	Crabapple 'Prairie Rose'	<i>Malus</i> 'Prairie Rose' <i>Prairie Rose Flowering Crab</i>	1.5"	BB/CONT	clump, 8' HGT
496	Perennials				
45	Daylily 'Bodacious Returns'	<i>Hemerocallis</i> 'Bodacious Returns'	#1	cont	
79	Daylily 'Joan Senior'	<i>Hemerocallis</i> 'Joan Senior'	#1	cont	
193	Grass 'Karl Foerster'	<i>Calamagrostis acutiflora</i> 'Karl Foerster'	#1	cont	
16	Panicum 'Dewey Blue'	<i>Panicum virgatum</i> 'Dewey Blue'	#1	cont	
59	Salvia 'East Friesland'	<i>Salvia nemerosa</i> 'East Friesland'	#1	cont	
45	Sedum 'Autumn Joy'	<i>Sedum spectabile</i> 'Autumn Fire'	#1	cont	
59	Yarrow 'Moonshine'	<i>Achillea millefolium</i> 'Moonshine'	#1	cont	
44	Blue Oat Grass 'Sapphire'	<i>Helictotrichon sempervirens</i> 'Sapphire'	#1	cont	
	Vines				
17	Boston Ivy	<i>Parthenocissus tricuspidata</i>	#1	cont	train to wall



- 5 PLANTING NOTES
SCALE= NTS



11/11/2016



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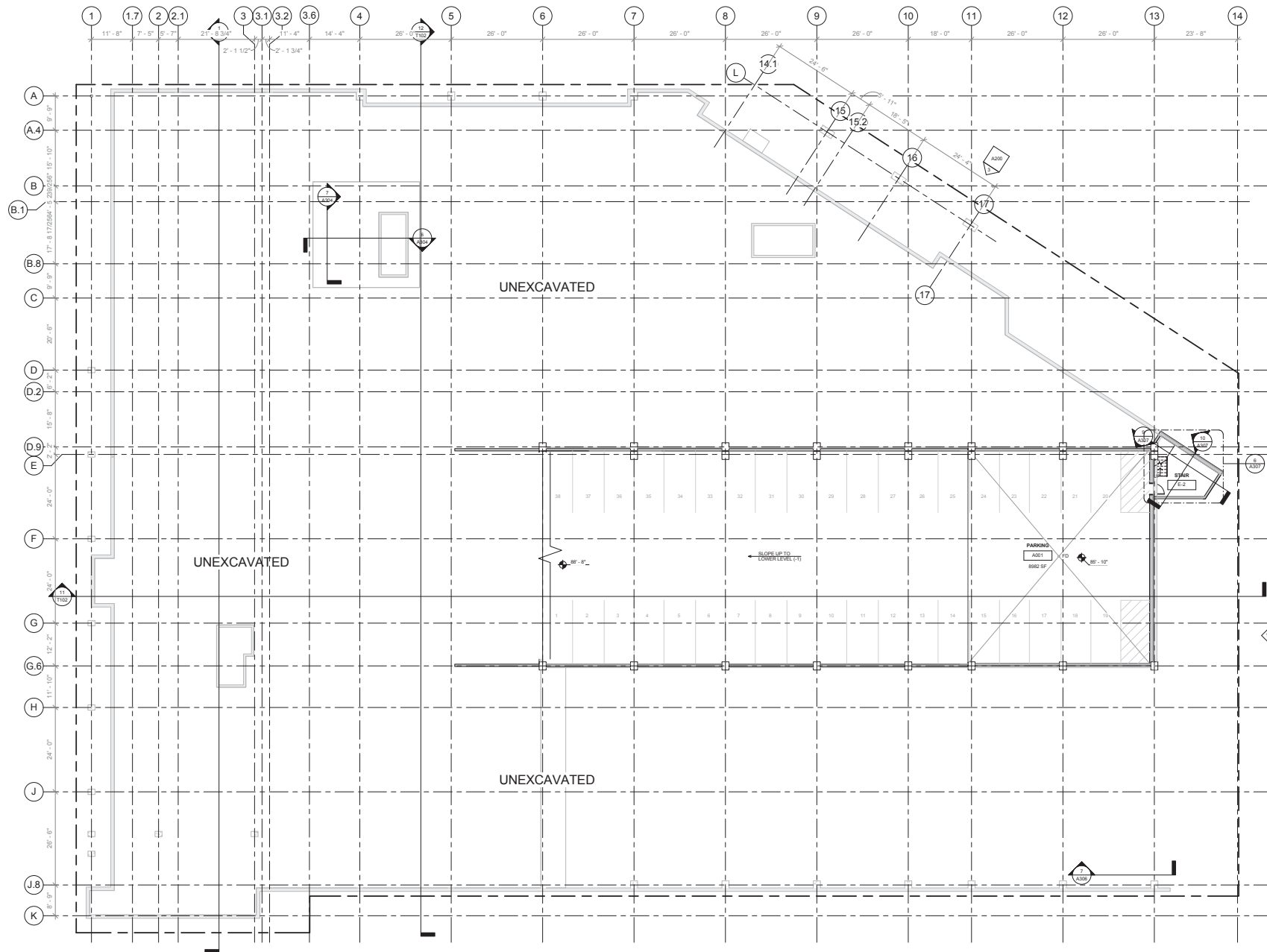
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AUGUST 08, 2016

PDR/ LUA
RE-SUBMIT
01-12-2017

SHEET TITLE

L300



1 SUB LEVEL (-2) FLOOR PLAN
A099 3/32" = 1'-0"



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DATE: 01/13/2017

PROGRESS

REVISIONS:

NO.	DATE	DESCRIPTION

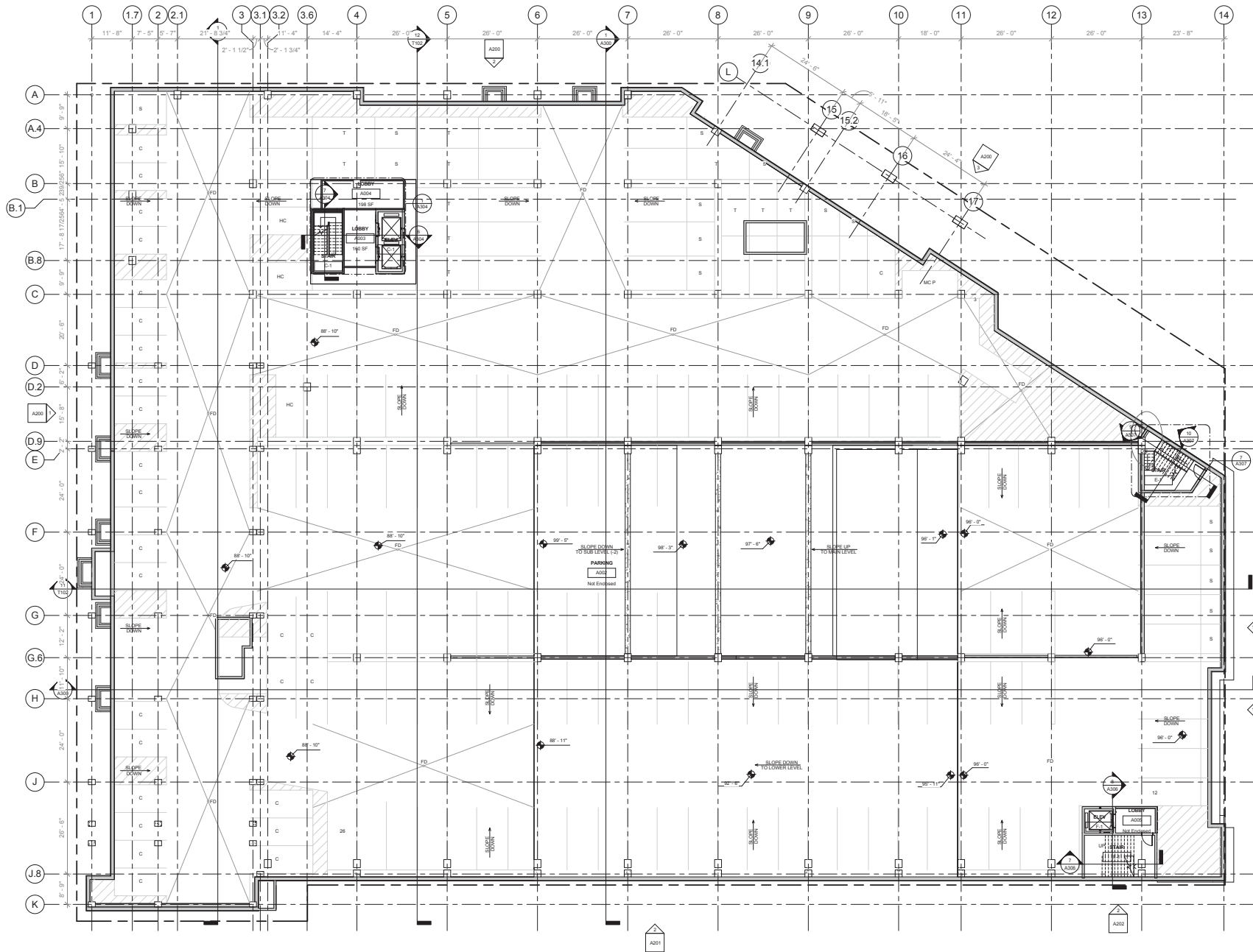
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CHECK BY: Checker

SHEET TITLE

SUB LOWER LEVEL
(-2) FLOOR PLAN

A099

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1 LOWER LEVEL (-1) FLOOR PLAN
A100 3/32" = 1'-0"



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DEVELOPMENT EXTENSIVE MANAGEMENT LLC

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612.382.0902

DATE: 01/13/2017

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REVISIONS:	DATE	DESCRIPTION

PROJECT NO: 33-XXXX
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SHEET TITLE

LOWER LEVEL (-1)
FLOOR PLAN

A100



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DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.

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DATE: 01/13/2017

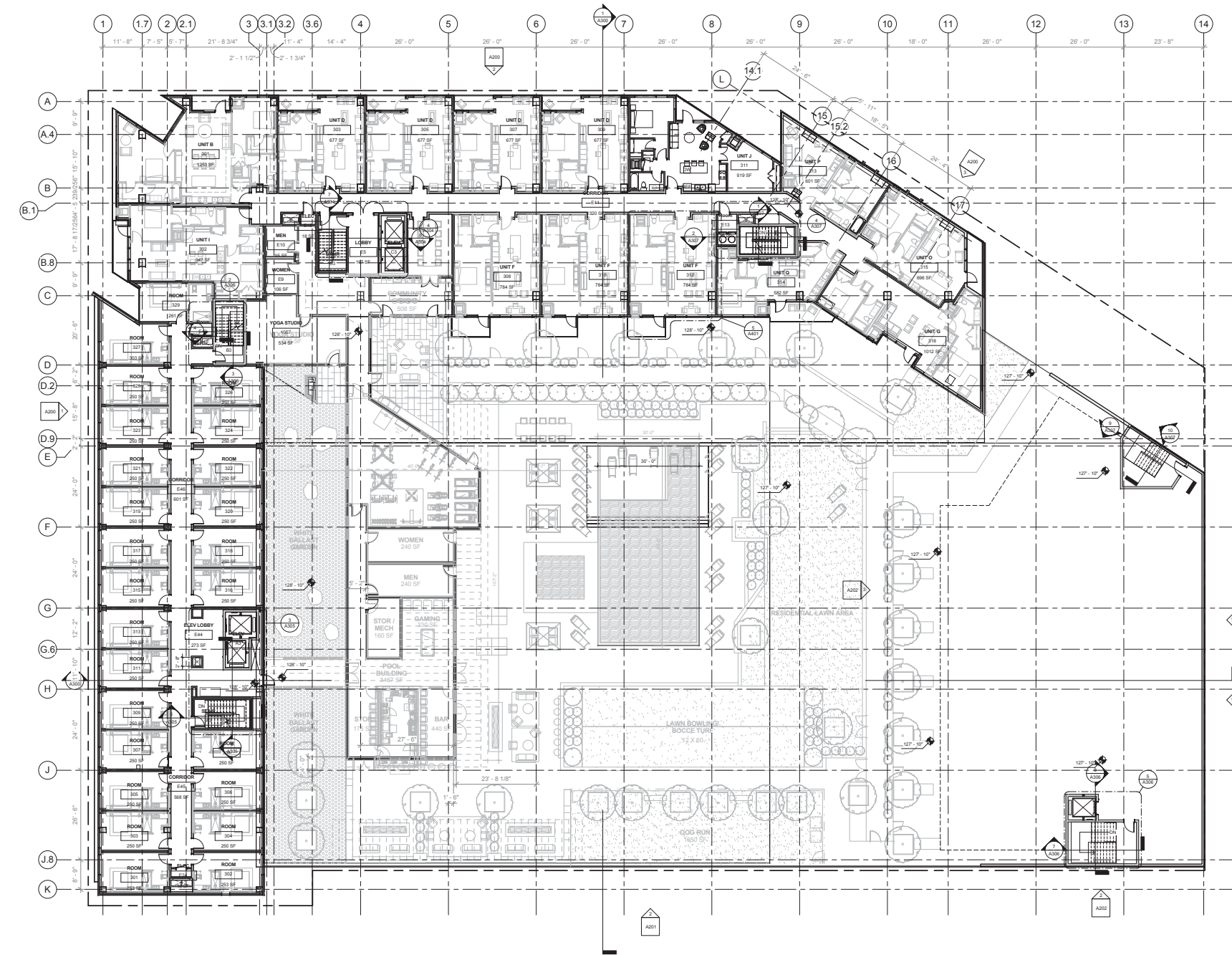
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1ST FLOOR PLAN

A101



1/13/2017 3:28:40 PM



1 3RD FLOOR PLAN
A103 3/32" = 1'-0"



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DATE: 01/13/2017

PROGRESS

REVISIONS:	DATE	DESCRIPTION

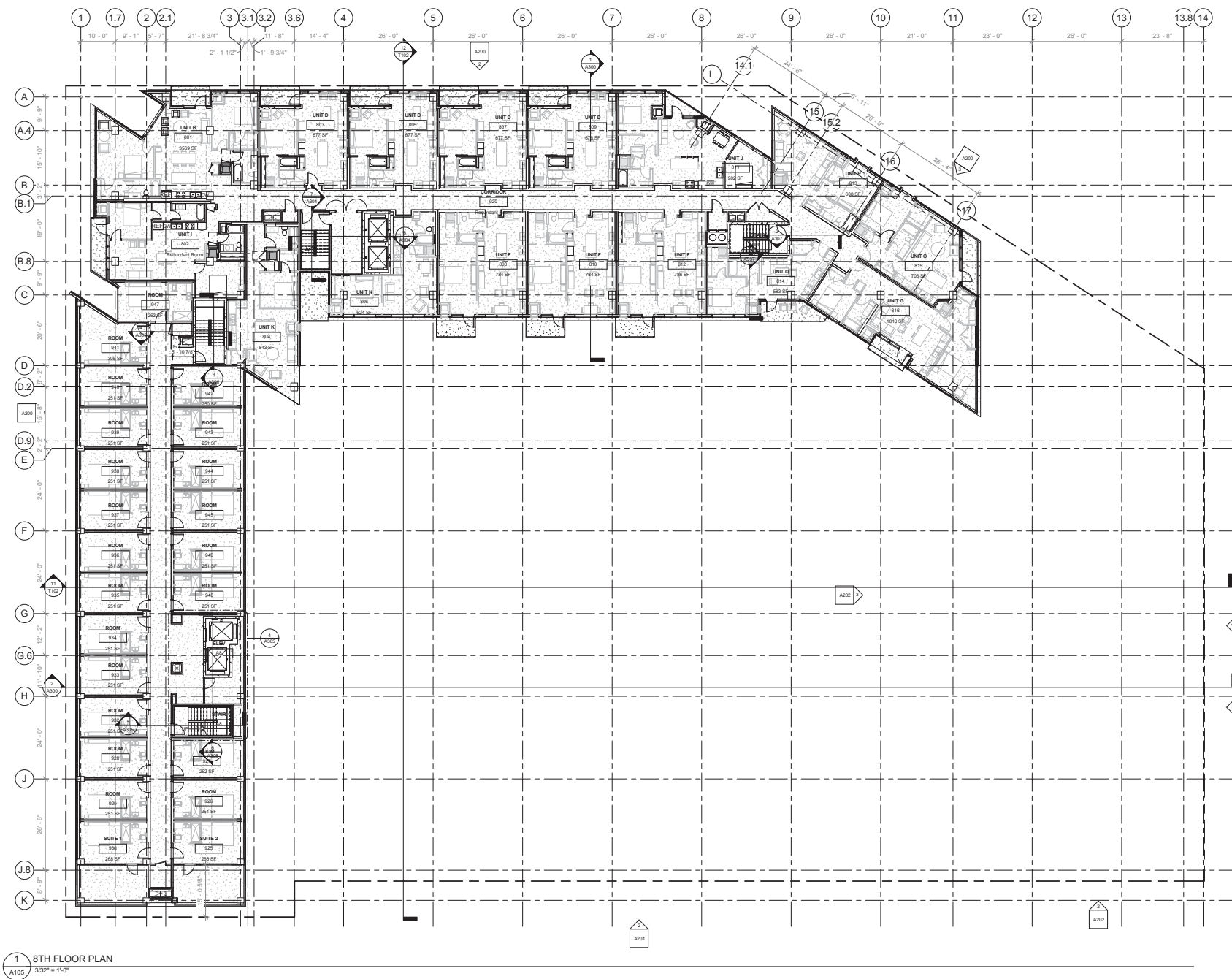
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SHEET TITLE

3RD FLOOR PLAN

A103



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DATE: 01/13/2017

PROGRESS

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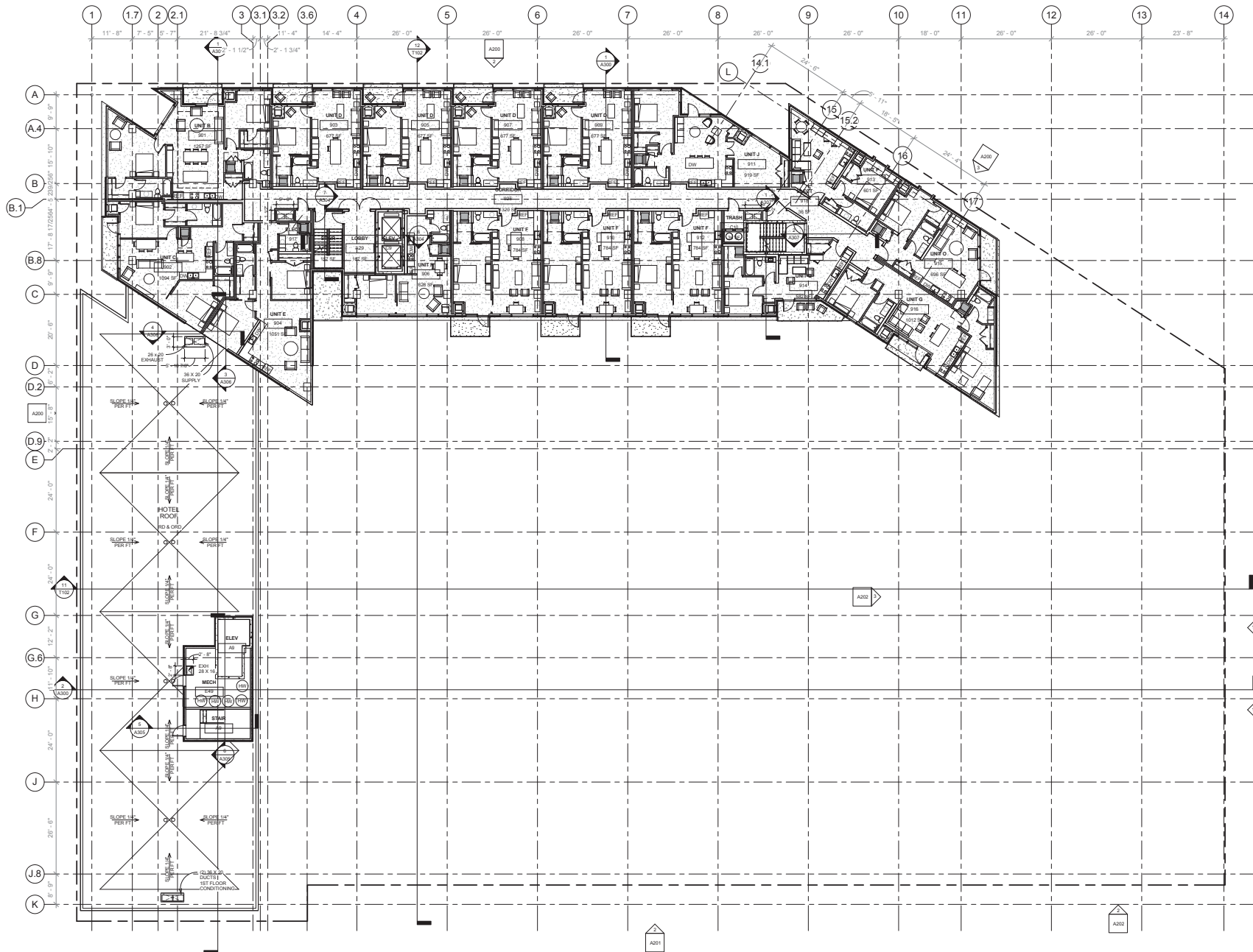
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SHEET TITLE

8TH FLOOR PLAN

A105



1 9TH FLOOR PLAN
3/32" = 1'-0"



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DATE: 01/13/2017

PROGRESS

REVISIONS:	DATE	DESCRIPTION

PROJECT NO: 33-XXXX
DRAWN BY: Author
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SHEET TITLE

9TH - 10TH FLOOR PLAN

A106



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LANDSCAPE ARCHITECT
BEN ERICKSON
612 382 0902

DATE: 01/13/2017

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PROJECT NO:	XX-XXXX
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SHEET TITLE

11TH - 12TH FLOOR
PLAN

A10 ☐



GRAVES HOSPITALITY
DEVELOPMENT EXPERTISE. MANAGEMENT SAVVY.

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1 A20

DATE: 01/13/2017

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SHEET TITLE

A10 ☐





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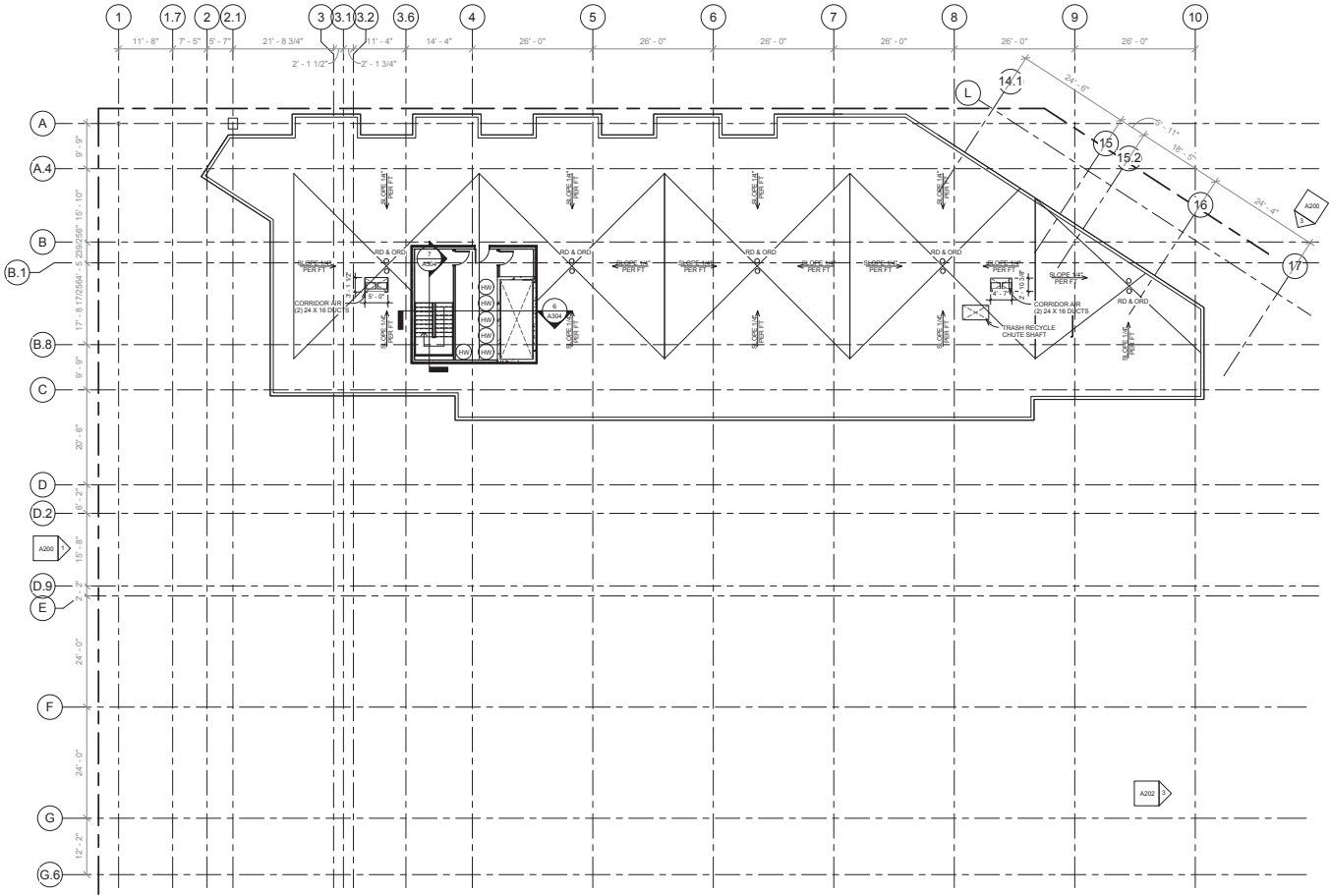
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REVISIONS:		
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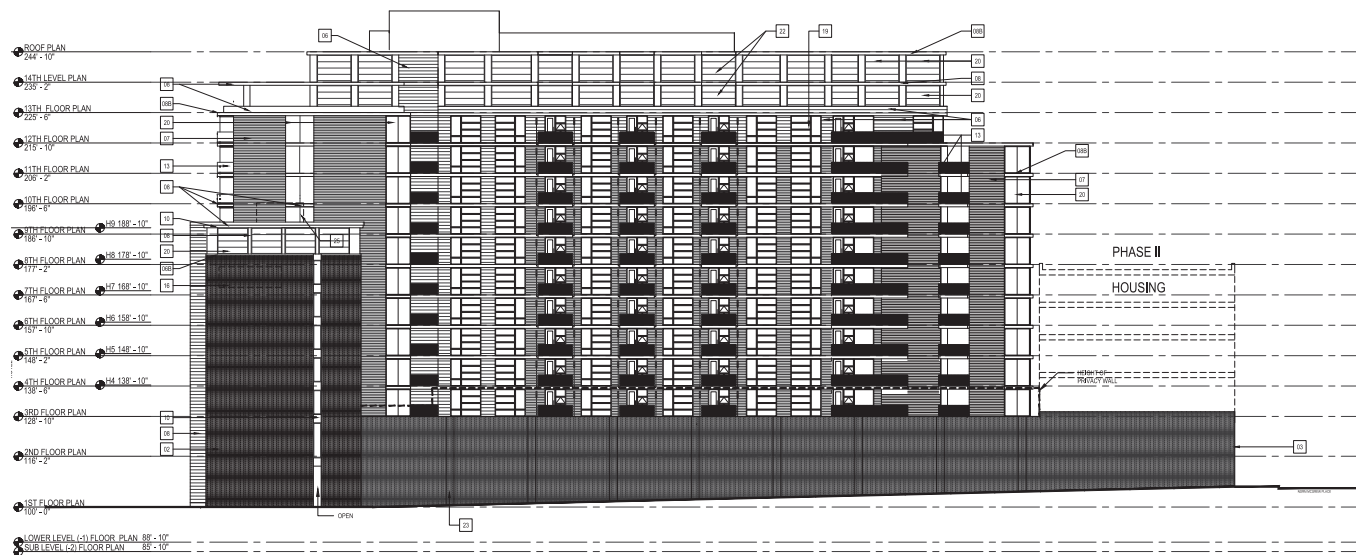
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ROOF PLAN

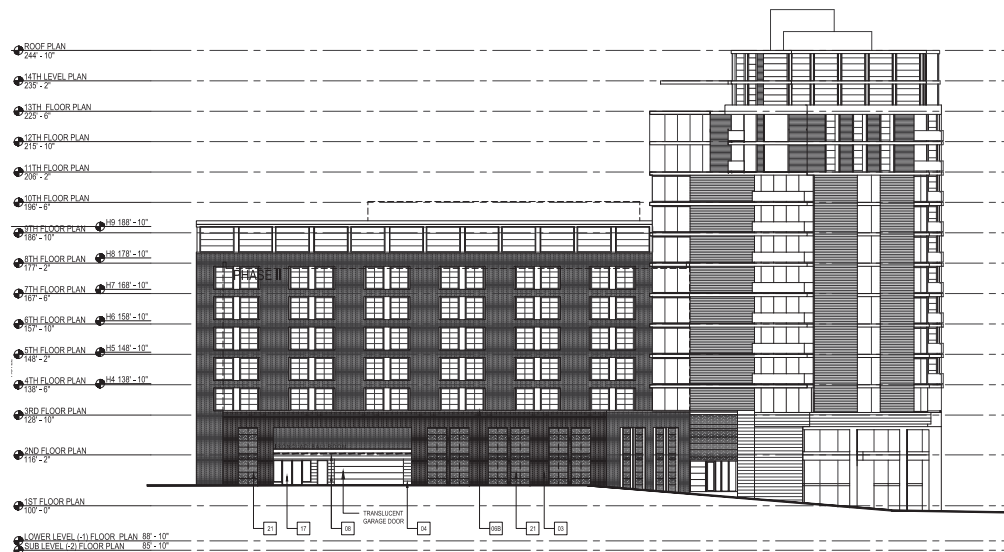
A110



1 ROOF PLAN
A110 3/32" = 1'-0"



② SOUTH ELEVATION
1/16" = 1'-0"



① EAST ELEVATION (NORM McGREW PLACE)
1/16" = 1'-0"

MATERIAL INDEX

01	SMALL METAL PANEL CONCRETE
02	SMALL METAL PANEL CONCRETE
03	SMALL METAL PANEL CONCRETE
04	SMALL METAL PANEL CONCRETE
05	SMALL METAL PANEL CONCRETE
06	SMALL METAL PANEL CONCRETE
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811 WASHINGTON AVENUE
MINNEAPOLIS, MN



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b.e. landscape designs
LANDSCAPE ARCHITECT
BEN ERICKSON
612.382.0902

DATE: 01.12.2017

PDR / LUA RESUBMIT

REVISIONS:	DATE	DESCRIPTION
1	01/12/17	ASB
2	01/12/17	PL
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SHEET TITLE
EXTERIOR ELEVATIONS



KHARBANDA MANAGEMENT



DATE: _____

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LANDSCAPE ARCHITECT
BEN ERICKSON
612.382.0902

DATE: 01.12.2017

PDR / LUA RESUBMIT

[illegible]

PROJECT NO:	Project Number
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DRAWN BY:	JJ/LS/AR
CHECKED BY:	PK

SHEET TITLE

EXTERIOR ELEVATIONS

A202

MATERIAL INDEX	
ALPHABETICALLY BY MATERIAL TYPE	
A. METALS AND METALLURGY	
01	NONHEATED BLACK STEEL
02	HEATED BLACK STEEL
03	HEATED ALUMINUM
04	NORMAN FACE BRICK
05	GLAZED BRICK
06	7" CRAMMIE PADE
07	CAST IN PLACE CONCRETE
08	CAST IN PLACE CONCRETE
09	CAST IN PLACE CONCRETE
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xxx WASHINGTON AVE

KARBANDA MANAGEMENT



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LANDSCAPE ARCHITECT
BEN ERICKSON
612.382.0902

DATE: 01/13/2017

PROGRESS

[illegible]

PROJECT NO:	XXX-XXXX
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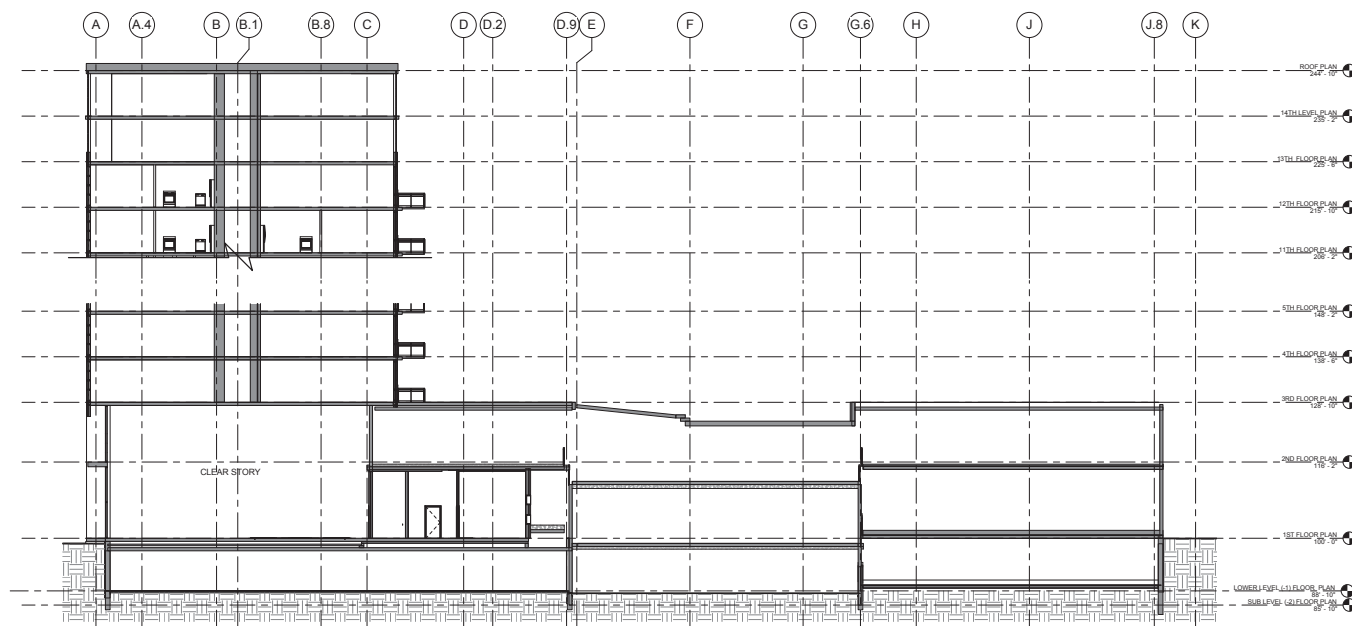
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CHECKED BY:	SK

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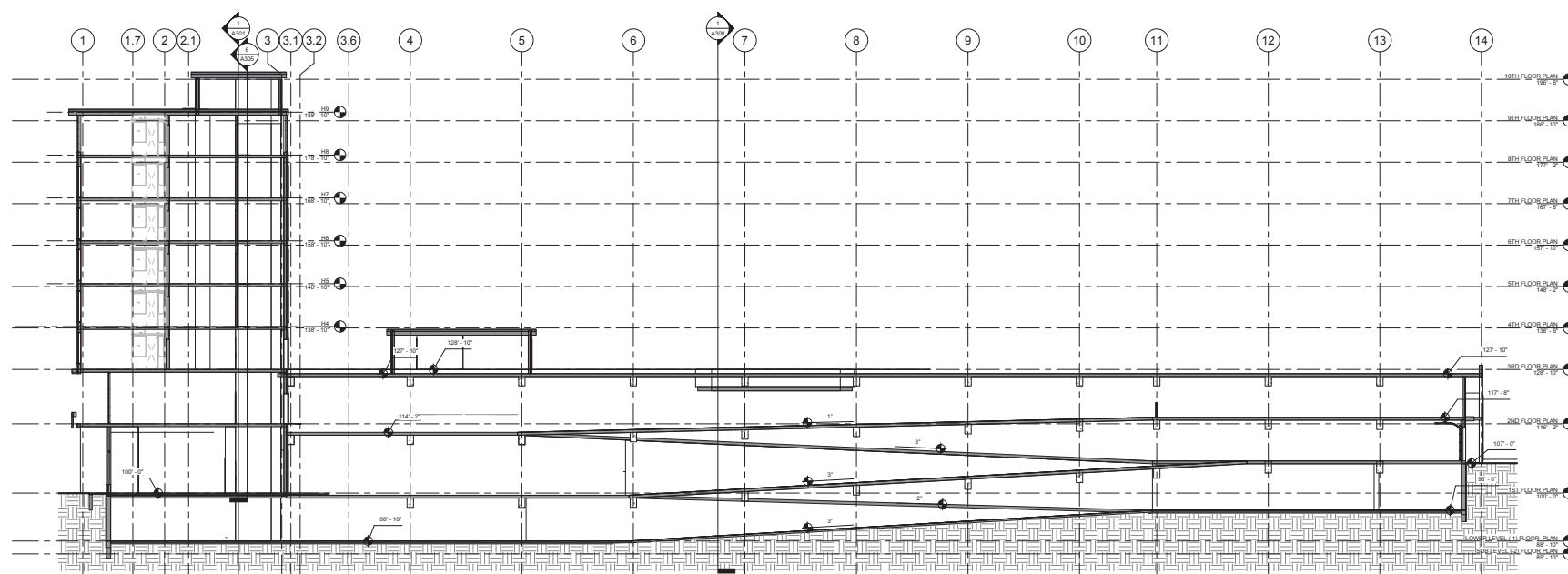
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BUILDING SECTION

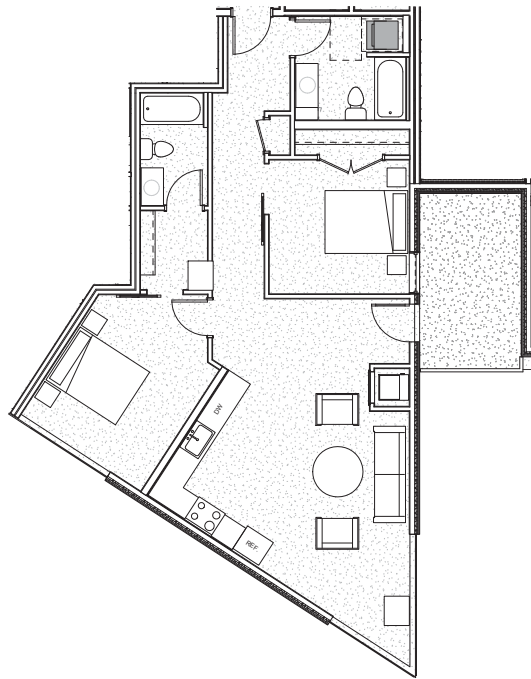
A300



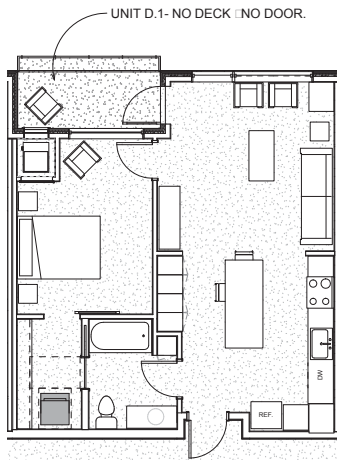
1 BUILDING SECTION THROUGH RESIDENCE
3/32" = 1'-0"



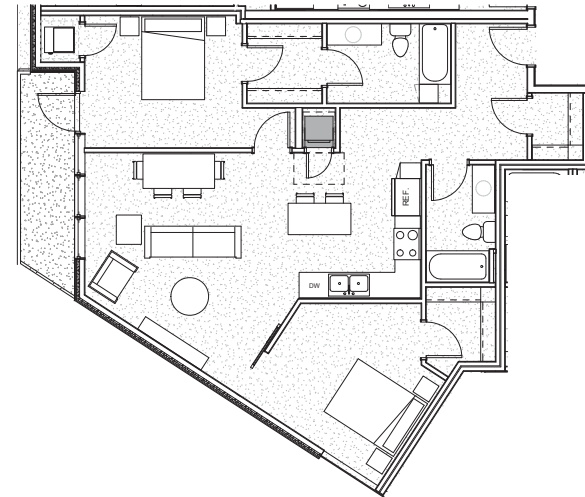
2 BUILDING THROUGH HOTEL
A300 3/32" = 1'-0"



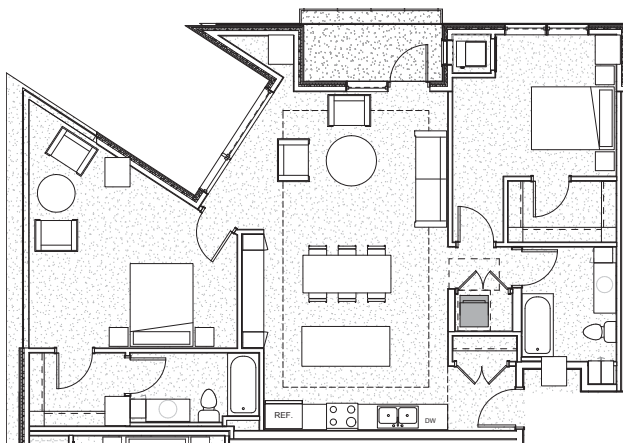
5 UNIT PLAN 'E'
A400 1/4" = 1'-0"



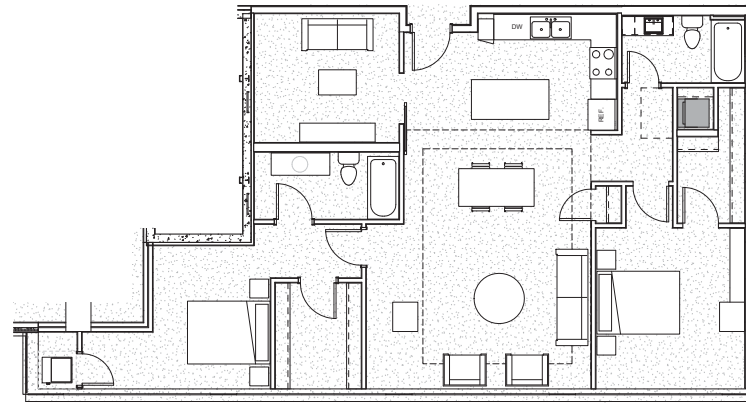
4 UNIT PLAN 'D'
A400 1/4" = 1'-0"



3 UNIT PLAN 'C'
A400 1/4" = 1'-0"



2 UNIT PLAN 'B'
A400 1/4" = 1'-0"



1 UNIT PLAN 'A'
A400 1/4" = 1'-0"



IRONCLAD
xxx WASHINGTON AVE

KHARBANDA MANAGEMENT
GH GRAVES HOSPITALITY
DEVELOPMENT EXPERTISE. MANAGEMENT SKILL.

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BEN ERIKSSON
612.382.0902

DATE: 01/13/2017

PROGRESS

REVISIONS:	DATE	DESCRIPTION

PROJECT NO: 33-XXXX
DRAWN BY: Author
CHECK BY: Checker

SHEET TITLE

**ENLARGED UNIT
PLANS**

A : 00



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DEVELOPMENT EXPERIENCE MANAGEMENT SERVICES

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PROGRESS

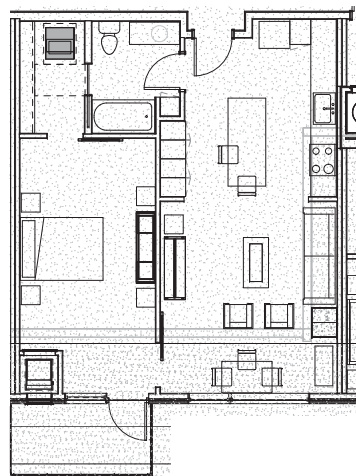
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CHECK BY: Checker

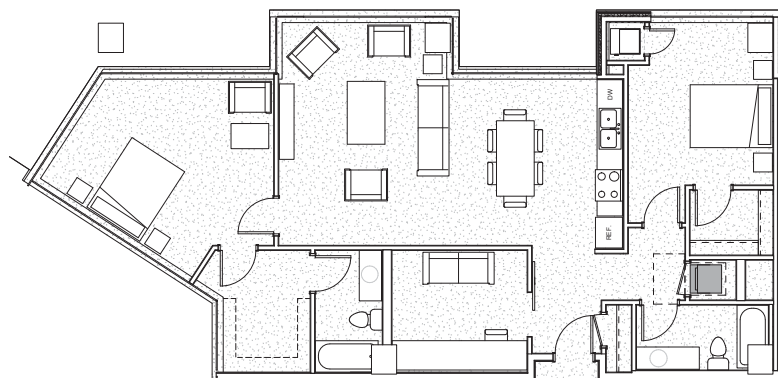
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**ENLARGED UNIT
PLANS**

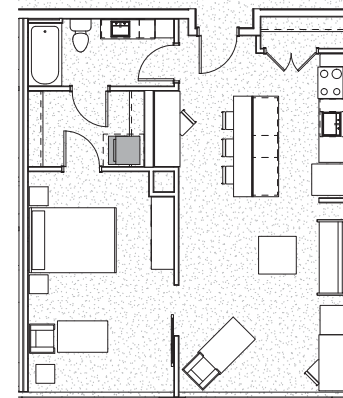
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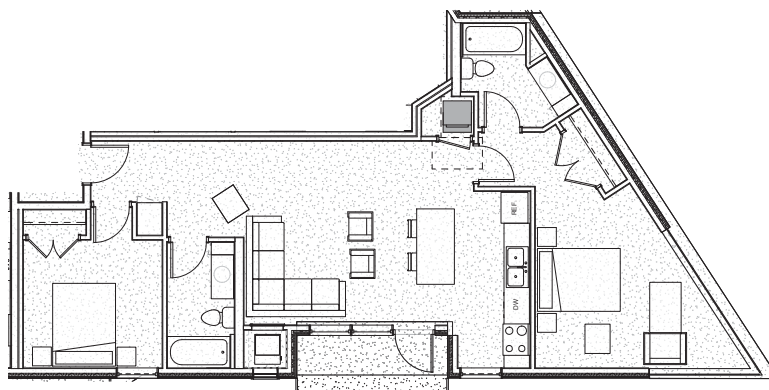
5 UNIT PLAN F.1
A401 1/4" = 1'-0"



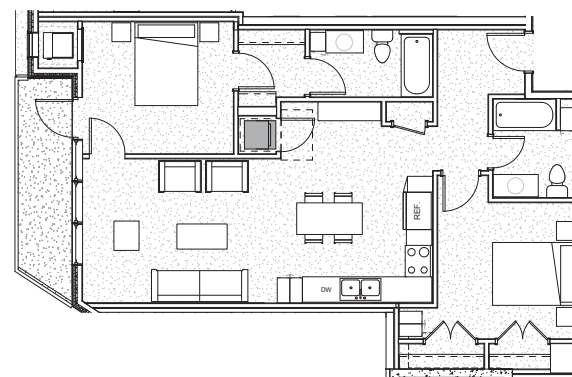
4 UNIT PLAN 'H'
A401 1/4" = 1'-0"



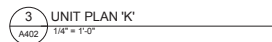
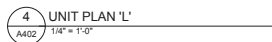
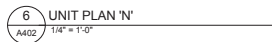
2 UNIT PLAN 'F'
A401 1/4" = 1'-0"

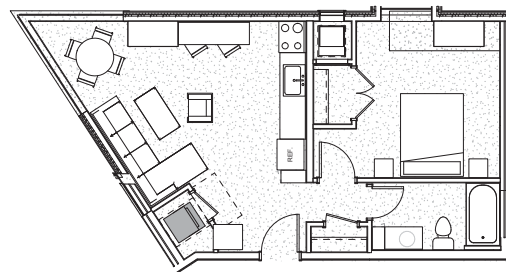


3 UNIT PLAN 'G'
A401 1/4" = 1'-0"

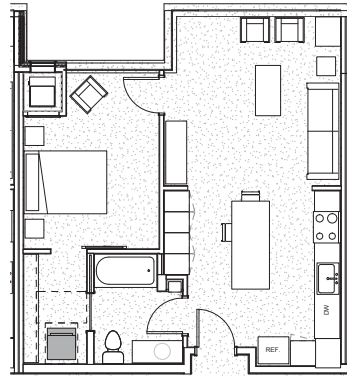


1 UNIT PLAN 'I'
A401 1/4" = 1'-0"

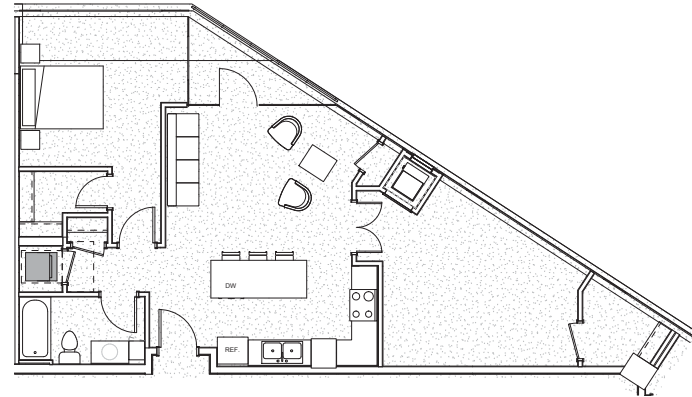




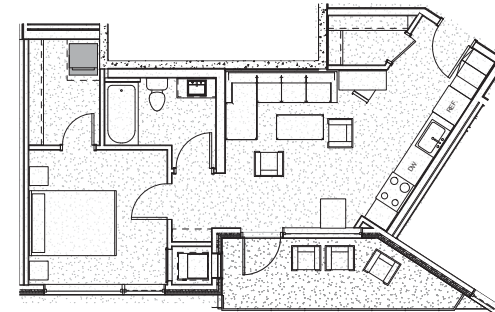
2 UNIT PLAN 'P'
A403 1/4" = 1'-0"



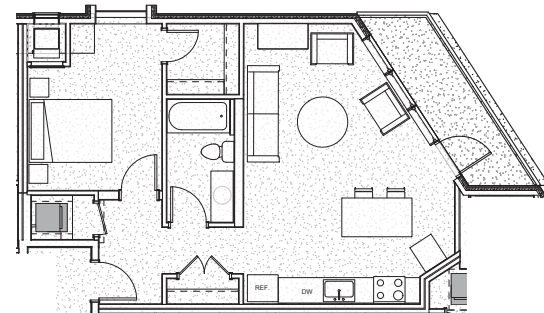
4 UNIT PLAN D.1
A403 1/4" = 1'-0"



5 UNIT PLAN 'H.1'
A403 1/4" = 1'-0"



3 UNIT PLAN 'I'
A403 1/4" = 1'-0"



1 UNIT PLAN 'O'
A403 1/4" = 1'-0"



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LANDSCAPE ARCHITECT
BEN ERICKSON
612.382.0902

DATE: 01/13/2017

PROGRESS

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO. XX-XXXX

DRAWN BY: Author

CHECK BY: Checker

SHEET TITLE

ENLARGED UNIT
PLANS

A 03



40 S. 7th Street, STE 212 PMB 172
Minneapolis, Minnesota 55402
Phone: (612) 659-1279
Online: www.thedmna.org

May 16, 2016

RE: DMNA Support for the Iron Clad

Dear Mr. Graves & Mr. Kharbanda,

The DMNA would like to extend its support for the Iron Clad development as presented on May 3rd, 2016 to the DMNA Land Use Committee, with a few conditions as outlined below.

The DMNA Land Use Committee believes that the Iron Clad will positively contribute to the architectural diversity of the area by integrating three towers, each with a distinct personality and opportunity for retail, into the site. The development honors the importance of the corner by activating street frontage on Chicago and Washington Avenue as well as McGrew Place. Maintaining the existing stonewall at the intersection of Chicago and Washington and the active use of McGrew place communicate the industrial history of the site. Leveling the site grade as well as the proposed setbacks on Washington will improve the pedestrian experience at the corner. The generous planned bike parking will serve to encourage guests and residents to travel to and from the site by bicycle.

The DMNA would like to see the following improvements to the design:

- The blank south wall parking structure is one of the proposed development's weakest features. It would be critical to incorporate a green wall or artistic screening, as this area will be in the direct line of sight for residents of Emmanuel Housing.
- The 33' curb cut on Chicago Avenue, without proper treatment, presents a hazard to pedestrians. Given the valuable pedestrian connection that Chicago will provide between the Commons/U.S. Bank Stadium and the Mississippi River, this pedestrian realm warrants special consideration. The developer should find an aesthetically pleasing way, perhaps through pavers, to communicate the pedestrian crossing-area to cars. During times of low-traffic, when the full curb cut is unnecessary, it would be highly preferable to narrow the opening through movable planters, artistic bollards or another creative solution (for which the Graves are famous!).
- Exterior and interior dog-relief areas should be provided to prevent damage to green space in public areas, as has become a nagging problem at Gold Medal Park and around residential buildings throughout the Mill District.
- The developer should maximize every opportunity to integrate trees and other greenery into the public realm.

If the project design changes significantly or construction does not commence by Summer 2017, the DMNA reserves the right to revisit its letter of support.

Regards,
Nick Cichowicz
DMNA Board Chair