

LAND USE APPLICATION SUMMARY

Property Location: 740 South 4th Street
Project Name: Block 1 Residential
Prepared By: Hilary Dvorak, Principal City Planner, (612) 673-2639
Applicant: Ryan Companies US, Inc.
Project Contact: Joe Peris with Ryan Companies US, Inc.
Request: To construct Phase V of a Planned Unit Development including a 342-unit residential building

Required Applications:

Conditional Use Permit for a Planned Unit Development	For Phase V of a Planned Unit Development including a 342-unit residential building.
Site Plan Review	For a new 342-unit residential building.
Registered Land Survey	To create separate ownership tracts of land.

SITE DATA

Existing Zoning	B4N Downtown Neighborhood District DP Downtown Parking Overlay District
Lot Area	18,226 square feet / .42 acres
Ward(s)	3
Neighborhood(s)	Downtown East
Designated Future Land Use	Mixed Use
Land Use Features	Transit Station Area (U. S. Bank Stadium) Growth Center (Downtown) Commercial Corridor (Chicago Avenue)
Small Area Plan(s)	<u>Historic Mills District Master Plan</u> (1998) <u>Update to the Historic Mills District Plan</u> (2001) <u>Downtown East/North Loop Master Plan</u> (2003)

Date Application Deemed Complete	February 1, 2019	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	April 1, 2019	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The site is located on South 4th Street between Park Avenue and Chicago Avenue. The site is currently vacant.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by office and commercial buildings, residential developments of varying densities, The Commons, US Bank Stadium and structured and surface parking lots. The site is located in the Downtown East neighborhood

PROJECT DESCRIPTION. Ryan Companies has redeveloped the majority of five blocks on the east side of Downtown Minneapolis over the course of the last six years. Three of the blocks are bounded by South 3rd Street, 5th Avenue South, South 4th Street and Chicago Avenue South and the two additional blocks are bounded by South 4th Street, 5th Avenue South, South 5th Street and Park Avenue. The entire development has been reviewed as a multi-phased Planned Unit Development (PUD).

PHASE I – Approved in 2013

The first phase of the PUD included two mixed-use buildings including office space, ground level and skyway level commercial space, residential units and enclosed parking and loading areas. The two buildings are mirror images of one another and are located between South 3rd Street, 5th Avenue South, South 4th Street and Park Avenue. This phase also included an above ground parking garage located on the block between South 3rd Street, Park Avenue, South 4th Street and Chicago Avenue. There are 1,500 parking spaces in this garage. Skyways connect the three new buildings to the existing Jerry Haaf parking garage, which connects to the extensive downtown skyway network and the new Minnesota Multi-Purpose Stadium. The first phase also includes a residential development on the western one-third of the block bounded by South 4th Street, 5th Avenue South, South 5th Street and Portland Avenue South.

PHASE II – Approved in 2015

The second phase of the PUD included the construction of a Radisson Red Hotel on the northern portion of the block bounded by South 3rd Street, Portland Avenue South, South 4th Street and Park Avenue. The hotel is five-stories tall and has 164 rooms. There is a bar/restaurant and fitness space on the ground floor of the building that is affiliated with the hotel. There is also additional commercial space not associated with the hotel on the ground-floor. There are 13 parking spaces provided towards the south side of the building and would connect to the adjacent underground parking garage via a tunnel for access to an additional 19 parking spaces.

PHASE III - Approved in 2015

The third phase of the PUD involved the construction of a park on the eastern two-thirds of the block bounded by South 3rd Street, Portland Avenue South, South 4th Street and Park Avenue and the block bounded by South 4th Street, Portland Avenue South, South 5th Avenue and Park Avenue. This is the park commonly referred to as The Commons.

PHASE IV – Approved in 2016

The fourth phase of the PUD involved the construction of an office building located on the northern half of the block bounded by South 3rd Street, 5th Avenue South, South 4th Street and Portland Avenue. The office building is approximately 221,000 square feet in size and four-stories tall. There is one level of underground parking containing 60 spaces. Access to the underground parking garage is from a shared ramp on 5th Avenue South that is also utilized by the residential units that were constructed as part of the first phase of the PUD. There are two

loading spaces located within the building. The loading spaces are located in the middle of the proposed building and accessed from the loading area within the office tower that was constructed as part of the first phase of the PUD. A tunnel connects the office building to the office tower that was constructed as part of the first phase of the PUD. This allows the users of the proposed office building to gain access to the city's skyway system.

PHASE V – Current proposal

The fifth phase of the PUD will be a 342-unit residential building located on the southern half of the block bounded by South 3rd Street, Park Avenue, South 4th Street and Chicago Avenue. The building will be approximately 320,000 square feet in size and 25 stories tall. There will be ground floor lobby and residential amenity space and rooftop amenity space provided in the building. Some of the ground floor amenity space could also be used as commercial tenant space depending on the market. There will also be a ground floor parking and loading area. Access to this space will be from South 4th Street. There will be 120 parking spaces in the building. Access to the parking will be from South 4th Street, Park Avenue and the adjacent parking garage. The building will connect to the city's skyway system.

RELATED APPROVALS.

Planning Case #	Application(s)	Description	Action
BZZ-6309 RLS-71	Conditional use permit Site plan review Registered land survey	Phase I of a Planned unit development that includes approximately 1.5 million square feet of office space, approximately 20,000 square feet of commercial space, 203 dwelling units and a principal parking facility	Approved, November 12, 2013
BZZ-7225 RLS-79	Conditional use permits Site plan review Registered land survey	Phase II of a Planned Unit Development for a hotel	Approved, July 20, 2015
BZZ-7417	Conditional use permit Site plan review	Phase III of a Planned Unit Development for a new park	Approved, October 19, 2015
BZZ-7527	Conditional use permit Site plan review	Phase IV of a Planned Unit Development for a four-story office building	Approved, January 11, 2016
BZZ-7524	Variance	Of the enclosed building standards to allow an ATM on the	Approved, January 25, 2017

		outside of both Wells Fargo Towers	
BZZ-7592	Conditional use permit	Modification to Phase II of a Planned Unit Development to allow two signs to exceed the maximum allowed height on the building	Approved, April 11, 2016
PLAN 3705	Variance	To increase the number of freestanding signs in The Downtown East Commons Park	Approved, March 20, 2017
PLAN 4347	Variances	For a wall sign on a non-primary building wall of the office building	Approved, June 12, 2017
PLAN 8228	Variance	For a roof sign on the office building	Approved, February 25, 2019

PUBLIC COMMENTS. Public comment letters are included in the report. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT - PLANNED UNIT DEVELOPMENT

The Department of Community Planning and Economic Development has analyzed the application to allow for Phase V of a Planned Unit Development including a 342-unit residential building based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment of Phase V of a PUD that includes a 342-unit residential building will not be detrimental to or endanger the public health, safety, comfort or general welfare. The proposed development will be built on land that has been vacant since 2013. The development will complete the multi-phased PUD.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The establishment of Phase V of a PUD that includes a 342-unit residential building will not be injurious to the use and enjoyment of other property in the vicinity or impede the normal or orderly development and improvement of surrounding property. Surrounding uses include office and commercial buildings, residential developments of varying densities, The Commons, US Bank Stadium and structured and surface parking lots. The proposed development will complement the existing uses in the area.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

In the B4N Downtown Neighborhood District, there is no minimum parking requirement for a multiple-family residential development and the maximum parking requirement is 1.6 spaces per dwelling unit. The applicant is proposing to provide 120 parking spaces within the building. Access to the enclosed parking spaces will be from South 4th Street, Park Avenue and the adjacent parking garage. The applicant has indicated that there will be a total of three vehicles provided within the building for shared use.

When parking is provided for multiple-family residential developments in the B4N Downtown Neighborhood District, guest parking at a ratio of one space per every 50 dwelling units is required. For 342 dwelling units, a total of seven guest parking spaces are required. The applicant is proposing to provide 120 parking spaces within the building but is not proposing to designate any for guests. The applicant is seeking an exception through the conditional use permit to reduce the amount of guest parking spaces provided in the building from seven to zero spaces.

The loading space requirement for the development is two small (10 feet by 25 feet) or one large space (12 feet by 50 feet). There will be two small loading spaces located within the building. The loading spaces will be accessed from South 4th Street.

The minimum bicycle parking requirement for the development is 171 spaces. The applicant has indicated that there will be bike storage areas within the building for up to 250 bicycles.

Being located in downtown Minneapolis, there are several transit options available. The Metro Transit Blue (Hiawatha) Line and the Metro Transit Green (Central Corridor) Line are located directly across the street from the proposed building. The Blue Line connects downtown Minneapolis to the Minneapolis-St. Paul International Airport and the Mall of America and the Green Line connects downtown Minneapolis to Downtown St. Paul. Both lines converge at the Downtown East Station which is located at Chicago Avenue and South 4th Street. In addition to light rail, there are several Metro Transit bus routes in the immediate area and there are numerous Nice Ride Minnesota stations located near the site. There are also numerous on-street designated bike lanes in the area.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the applicable policies of *The Minneapolis Plan for Sustainable Growth*. The property is designated as mixed use on the future land use map. The site is located in a designated Transit Station Area (U. S. Bank Stadium), within a Growth Center (Downtown) and near a Commercial Corridor (Chicago Avenue). In designated Activity Centers and Growth Centers, high density (50-120 du/acre) and very-high density (120-200 du/acre), is allowed dependent on context. Densities up to 800 du/acre may be allowed in or near all designated Growth Centers and within Activity Centers adjacent to Growth Centers, as consistent with adopted small area plans.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- 1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- 1.3.3 Encourage above-ground structured parking facilities to incorporate development that provides active uses on the ground floor.

Land Use Policy 1.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

- 1.13.5 Concentrate highest densities and mixed-use development adjacent to the transit station and along connecting corridors served by bus.

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

- 1.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

Urban Design Policy 10.1: Promote building designs and heights that enhance and complement the image and form of the Downtown skyline, provide transition to the edges of Downtown and protect the scale and quality in areas of distinctive physical or historical character.

- 10.1.1 Concentrate the tallest buildings in the Downtown core.
- 10.1.2 Building placement should preserve and enhance public view corridors that focus attention on natural or built features, such as landmark buildings, significant open spaces or water bodies.
- 10.1.3 Building placement should allow light and air into the site and surrounding properties.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.3 Large-scale, high-rise, multi-family residential development is more appropriate in the Downtown Minneapolis Growth Center.

- 6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the B4N Downtown Neighborhood District and the DP Downtown Parking Overlay District.

Findings Required for Planned Unit Developments:

- A. The planned unit development conforms to the applicable standards for alternatives and amenities. All planned unit developments shall provide at least one amenity or a combination of amenities that total at least 10 points, beyond those required for any alternatives. For each alternative requested, amenities shall total at least five points.

Alternatives requested:

Bulk regulations, building height.

In the B4N Downtown Neighborhood District the height limitation is 10 stories or 140 feet. The height of the building is proposed to be 27 stories or 305 feet in height. The applicant is proposing to provide five points worth of amenities to allow an increase in the maximum height of the building.

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

Increasing the height of the proposed building should have a minimal impact on the amount of light and air that surrounding properties receive. Public right-of-way separates the proposed development from surrounding properties to the south and west and to the north and east the development site abuts a parking garage.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant submitted a shadow study depicting shadowing impacts at 9 am, 12 noon and 3 pm on the Spring and Fall equinox and on the Summer and Winter solstice. The shadow study indicates that there will be shadows cast on surrounding residential buildings; however, the shadowing impacts do not appear to be significant given the context of the area. Surrounding properties would not be continually shadowed as the shadow would rotate from north to south throughout the day as expected. In addition, staff is not aware of any existing solar energy systems that would be affected by the proposed development.

3. *The scale and character of surrounding uses.*

The scale and character of the buildings as well as the architectural styles of the surrounding properties in this area are varied. There are a mix of uses that surround the subject site including office and commercial buildings, residential developments of varying densities, The Commons, US Bank Stadium and structured and surface parking lots. Many of the existing buildings in the area are turn of the 20th century warehouse buildings. However, there have also been many new buildings constructed in the surrounding area in the last five years. The height of surrounding buildings varies from two stories to towers that are approximately 300 feet tall. The proposal is compatible with the scale and character of other buildings in the area given the variation.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The proposed building will not block views of any landmark buildings, significant open spaces or water bodies.

Off-street parking and loading.

When parking is provided for multiple-family residential developments in the B4N Downtown Neighborhood District, guest parking at a ratio of one space per every 50 dwelling units is required. For 342 dwelling units, a total of seven guest parking spaces are required. The applicant is proposing to provide 120 parking spaces within the building but is not proposing to designate any for guests. The applicant would like to have the flexibility to offer the parking stalls that are able to fit on the site to the tenants, whether it be residential or commercial. The site is adjacent to a public parking garage. Given this, there is an opportunity for guests to utilize those parking spaces for their temporary parking needs. The applicant is not required to provide amenity points for this exception to the zoning code.

Minimum and maximum width of driveways.

The zoning code requires that two-way drive aisles have a minimum width of 22 feet. The drive aisles within the building range from 18 feet to 22 feet. Given the limited number of parking spaces provided per floor, and the fact that only residents of the building will be utilizing them, maneuvering into and out of the parking spaces from a drive aisle that is less than 22 feet will be adequate. The applicant is not required to provide amenity points for this exception to the zoning code.

Points required for alternatives:

- Bulk regulations, height – **5 points**.
- Off-street parking and loading – **0 points**.
- Minimum and maximum width of driveways – **0 points**.

Total = 5 points.

Phasing plan.

Phase	Approval Date	Alternative	Amenity
Phase I	November 12, 2013	Establishment of the PUD Building height On-premise signs	LEED Plaza Energy Efficiency Living wall system Shared vehicle Enhanced landscaping Enhanced stormwater management Recycling storage area
Phase II	July 20, 2015	Not applicable	Not applicable
Phase III	October 19, 2015	Not applicable	Not applicable
Phase IV	January 11, 2016	Not applicable	Not applicable
Phase V	March 11, 2019	Building height Off-street parking Minimum drive aisle width	Shared vehicles Recycling storage area Pet exercise area

Amenities provided:

The applicant is proposing the following amenities from Table 527-1, Amenities:

Shared vehicles (3 points)

Access to a shared passenger automobile available for short-term use. For residential uses, a minimum of one (1) car per one hundred (100) dwelling units is required.

For 342 dwelling units, a total of three shared vehicles are required to be provided on site. The applicant has indicated that there will be a total of three vehicles provided within the building for shared use.

Recycling storage area (1 point)

Provide an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building.

The applicant has indicated that the proposed building will have easily accessible dedicated rooms for trash and recycling storage that serve the entire building. The rooms will be dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals.

Amenities proposed by the applicant or others (1 point)

The city planning commission may consider other amenities not listed in Table 527-1, Amenities, that are proportionally related to the alternative requested. The commission may assign one (1), three (3), five (5), or ten (10) points based on the proportionality.

Pet exercise area

The applicant is proposing to provide a pet exercise area that does not meet the standards in Table 527-1, Amenities. The zoning code requires that the pet exercise area have a minimum dimension of 12 feet by 60 feet (720 square feet), that it is enclosed with decorative fencing, that it include lighting in compliance with Chapter 535, Regulations of General Applicability and that accommodations for proper disposal of animal waste be provided. In addition, the pet exercise area shall not be located in a required yard.

The applicant is proposing a pet exercise area on grade located towards the southeast corner of the site. The proposed pet exercise area is approximately 30 feet by 30 feet (900 square feet) in size. It would be enclosed with a five-foot tall decorative metal fence and would have lighting and accommodations for proper disposal of animal waste. The applicant is also proposing to provide bench seating in the space.

Amenity	Points Requested by Applicant	Points Recommended by Staff
Shared vehicles	3 points	3 points
Recycling Storage Area	1 point	1 point
Pet exercise area	1 point	1 point
	5 points	5 points

B. The city planning commission may authorize additional uses, subject to the following standards:

Not applicable.

C. The planned unit development conforms to the required findings for a planned unit development:

1. *That the planned unit development complies with all of the requirements and the intent and purpose of this chapter. In making such determination, the following shall be given primary consideration:*

a) *The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.*

Surrounding uses include office and commercial buildings, residential developments of varying densities, The Commons, US Bank Stadium and structured and surface parking lots. The proposed development will complement the existing uses in the area.

b) *The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provision of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

The applicant is proposing to provide 120 parking spaces within the building. Access to the enclosed parking spaces will be from South 4th Street, Park Avenue and the adjacent parking garage. The applicant has indicated that there will be a total of three vehicles provided within the building for shared use. There will be two small loading spaces located within the building. The loading spaces will be accessed from

South 4th Street. The minimum bicycle parking requirement for the development is 171 spaces. The applicant has indicated that there will be bike storage areas within the building for up to 250 bicycles.

Being located in downtown Minneapolis, there are several transit options available. The Metro Transit Blue (Hiawatha) Line and the Metro Transit Green (Central Corridor) Line are located directly across the street from the proposed building. The Blue Line connects downtown Minneapolis to the Minneapolis-St. Paul International Airport and the Mall of America and the Green Line connects downtown Minneapolis to Downtown St. Paul. Both lines converge at the Downtown East Station which is located at Chicago Avenue and South 4th Street. In addition to light rail, there are several Metro Transit bus routes in the immediate area and there are numerous Nice Ride Minnesota stations located near the site. There are also numerous on-street designated bike lanes in the area.

- c) *The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

The site amenities include shared vehicles, a recycling storage area and a pet exercise area.

- d) *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development, and protection of views and corridors.*

The scale and character of the buildings as well as the architectural styles of the surrounding properties in this area are varied. Many of the existing buildings in the area are turn of the 20th century warehouse buildings. However, there have also been many new buildings constructed in the surrounding area in the last five years. The height of surrounding buildings varies from two stories to towers that are approximately 300 feet tall. The exterior of the building will be clad in glass, precast concrete and metal. Buildings in the surrounding area are made out of brick, stone, concrete and metal. The proposed building will be compatible with the scale and character of other buildings in the area.

- e) *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing, and applicable policies of the comprehensive plan and adopted small area plans.*

There are no residential uses or residentially zoned properties adjacent to the site.

- f) *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

- g) *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

No buildings will be demolished as part of this development as the site is vacant. During construction, waste providers are selected that have off-site recycling centers.

2. *That the planned unit development complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.*

The applicant is proposing to subdivide the property into two lots; one for the existing parking garage and one for the proposed building. The proposed Registered Land Survey will meet the requirements of Chapter 598, Land Subdivision Regulations.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

Applicable Standards of Chapter 530, Site Plan Review

BUILDING PLACEMENT AND DESIGN

Building placement – Requires alternative compliance

- The first floor of the building is located between three and ten feet from the front lot line along South 4th Street and between three-and-a-half and five-and-a-half feet from the corner side property line along Park Avenue. Alternative compliance is required.
- The placement of the building reinforces the street wall, maximizes natural surveillance and visibility and facilitates pedestrian access and circulation.
- The area between the building and lot line includes landscaping.
- All on-site accessory parking is located within the principal building served.

Principal entrances – Meets requirements

- The building is oriented so that at least one principal entrance faces the front property line.
- The principal entrance is clearly defined and emphasized through the use of a glass canopy and signage.

Visual interest – Meets requirements

- The building walls provide architectural detail and contain windows in order to create visual interest.
- The proposed building emphasizes architectural elements – including recesses, projections, windows and entries – to divide the building into smaller identifiable sections.
- There are no blank, uninterrupted walls exceeding 25 feet in length.

Exterior materials – Meets requirements

- The applicant is proposing colored architectural precast, metal panel and glass as the building's primary exterior materials. Each elevation would comply with the City's durability standards for exterior materials (see Table 1). Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- The development is consistent with the City's policy of allowing no more than three exterior materials per elevation, excluding windows, doors and foundation materials.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.
- The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.

Table 1. Percentage of Exterior Materials per Elevation

Material	Allowed Max	North	South	East	West
Glass	100%	59%	56%	42%	54%
Concrete	100%	27%	33%	45%	30%
Metal Panel	75%	14%	11%	12%	16%

Windows – Meets requirements

- For residential uses, the zoning code requires that no less than 20 percent of the walls on the first floor, and no less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. The project is in compliance with the minimum window requirement (see Table 2).
- For nonresidential uses, the zoning code requires that no less than 30 percent of the walls on the first floor are windows with clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher (see Table 3).
- All windows are vertical in proportion and are evenly distributed along the building walls.

Table 2. Window Requirements for Residential Uses

	Code		Proposed	
1st floor facing South 4 th Street	20% minimum	275 sq. ft.	51%	704 sq. ft.
2nd floor and above facing South 4 th Street	10% minimum	222 sq. ft.	More than 10%	
1st floor facing Park Avenue	20% minimum	114 sq. ft.	3%	192 sq. ft.
2nd floor and above facing Park Avenue	10% minimum	71 sq. ft.	More than 10%	

Table 3. Window Requirements for Non-Residential Uses

	Code		Proposed	
1st floor facing South 4 th Street	30% minimum	120 sq. ft.	64%	256 sq. ft.

Ground floor active functions – Requires alternative compliance

- The ground floor facing South 4th Street contains 78 percent (173 feet) active functions and the ground floor facing Park Avenue contains 68 percent (48 feet) active functions. Alternative compliance is required.

Roof line – Meets requirements

- The principal roof line of the building will be flat which is similar to that of surrounding buildings.

Parking garages – Meets requirements

- The proposed parking garage complies with the minimum ground floor active functions requirements.
- Above the ground floor, the parking garage does not contain more than 30 percent of the linear frontage of each floor facing a public street, public sidewalk, or public pathway.

- Vehicles and internal garage lighting would be screened as viewed from the public right-of-way and nearby properties.
- The top level of the above-grade parking garage is enclosed.
- The applicant is proposing a building design that would cover and diminish the visibility of any sloping floor.
- The combined, above-grade floor area of the accessory parking garage does not exceed the gross floor area of all other uses located on the same zoning lot.
- The applicant is proposing ground floor lobby and residential amenity space between the ground floor of the parking garage and the public sidewalk except where frontage is needed to provide vehicular and pedestrian access to the facility.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- There are clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.

Transit access – Not applicable

- No transit shelters are proposed as part of this development.

Vehicular access – Meets requirements

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses.
- There will be two curb cuts leading to the site. The curb cut along South 4th Street will be used to access the ground floor parking and loading area and the curb cut along Park Avenue will be used to access the below grade parking.
- There are no public alleys adjacent to the site.
- Service vehicle access does not conflict with pedestrian traffic.
- There is no maximum impervious surface requirement in the B4N Downtown Neighborhood District. According to the materials submitted by the applicant, 94 percent of the site will be impervious, while 13 percent of the existing site is impervious.

LANDSCAPING AND SCREENING

General landscaping and screening – Not applicable

- In the Downtown Districts, any building containing 50,000 square feet or more of gross floor area is exempt from the general landscaping and screening requirements.

Table 3. Landscaping and Screening Requirements

		Code	Proposed
Lot Area		--	18,226 sq. ft.
Building Footprint		--	15,653 sq. ft.
Remaining Lot Area		--	2,573 sq. ft.
Landscaping Required		0 sq. ft.	1,081 sq. ft.
Canopy Trees (1:500 sq. ft.)		0 trees	4 trees

Shrubs (1:100 sq. ft.)		0 shrubs	29 shrubs
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Parking and loading landscaping and screening – *Not applicable*

- There is no surface parking proposed for the site, so the site is not subject to the screening and landscaping requirements for parking areas per section 530.170.

Additional landscaping requirements – *Meets requirements*

- Information included in the landscape plan indicates that the plant materials, and installation and maintenance of the plant materials, would comply with sections 530.200 and 530.210 of the zoning code.
- All other areas not occupied by buildings, parking and loading facilities, or driveways would be covered with turf grass, native grasses, perennials, wood mulch, shrubs and trees.

ADDITIONAL STANDARDS**Concrete curbs and wheel stops – *Not applicable***

- There are no surface parking spaces proposed on the site.

Site context – *Meets requirements*

- The proposed building will not block views of any landmark buildings, significant open spaces or water bodies.
- The applicant submitted a shadow study depicting shadowing impacts at 9 am, 12 noon and 3 pm on the Spring and Fall equinox and on the Summer and Winter solstice. The shadow study indicates that there will be shadows cast on surrounding residential buildings; however, the shadowing impacts do not appear to be significant given the context of the area. Surrounding properties would not be continually shadowed as the shadow would rotate from north to south throughout the day as expected. In addition, staff is not aware of any existing solar energy systems that would be affected by the proposed development.
- This building has been designed to minimize the generation of wind currents at ground level. The glass canopy over the principal entrance and the projecting balconies will help reduce wind currents coming off of the building.

Crime prevention through environmental design – *Meets requirements*

- The site plan employs best practices to increase natural surveillance and visibility, to control and guide movement on the site and to distinguish between public and non-public spaces.
- The proposed site, landscaping and building promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- The project provides lighting on site, at all building entrances and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.
- The landscaping, sidewalks, lighting, fencing and building features are located to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- The entrances, exits, signs, fencing, landscaping and lighting are located to distinguish between public and private areas, to control access and to guide people coming to and going from the site.

Historic preservation – *Not applicable*

- This site is neither historically designated or located in a designated historic district, nor has it been determined to be eligible for designation.

Applicable Regulations of the Zoning Ordinance

The proposed use is conditional in the B4N Downtown Neighborhood District.

Off-street Parking and Loading – Requires conditional use permit

- The applicant is seeking an exception through the conditional use permit to reduce the amount of guest parking spaces provided in the building from seven to zero spaces (see Table):

Table 4. Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Overall Minimum	Maximum Allowed	Proposed
Residential Dwellings	0	--	0	547	120
Guest parking	7	--	7	7	0
	7	--	7	554	120

Table 5. Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	171	--	Not less than 90%	250
	171	--	154	250

Table 6. Loading Requirements (Chapter 541)

Use	Loading Requirement	Loading Spaces	Proposed
Residential Dwellings	2 Small and 1 Large	2 Small	2 Small
	2 Small and 1 Large	2 Small	2 Small

Building Bulk and Height – Requires conditional use permit

- The applicant is seeking an exception through the conditional use permit to increase the height of the building from 10 stories or 140 feet to 27 stories or 305 feet (see Table 7):

Table 7. Building Bulk and Height Requirements

	Code	Bonuses	Total	Proposed
Lot Area	--	--	--	18,226 sq. ft. / .42 acres
Gross Floor Area	--	--	--	319,475 sq. ft.
Min. Floor Area Ratio	2.0	--	--	17.53
Max. Floor Area Ratio	--	--	--	17.53
Max. Building Height	10 stories or 140 feet, whichever is less		10 stories or 140 feet, whichever is less	27 stories or 305 ft.

Lot and Residential Unit Requirements – Meets requirements**Table 8. Lot and Residential Unit Requirements Summary**

	Code	Proposed
Min. Lot Area	1 acre	18,226 sq. ft. 12.55 acres (total PUD)
Min. Lot Width	--	254 ft.
Max. Impervious Surface Area	--	94%
Max. Lot Coverage	--	86%
Dwelling Units (DU)	--	342 DUs
Density (DU/acre)	--	814 DU/acre

Yard Requirements – Meets requirements**Table 9. Minimum Yard Requirements**

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (South 4th Street)	0 ft.	--	0 ft.	Between 3 and 10 ft.
Interior Side (North)	0 ft.	--	0 ft.	0 ft.
Interior Side (East)	0 ft.	--	0 ft.	Between 0 and 30 ft.
Corner side (Park Avenue)	0 ft.	--	0 ft.	Between 3.5 and 5.5 ft.

Signs – Meets requirements

- All signs are subject to Chapter 543, On-Premise Signs. The applicant will be required to submit a separate sign permit application for any signage that is proposed.
- In the B4N Downtown Neighborhood District there can be two-and-a-half square feet of signage for every one linear foot of primary building wall. Wall signs are limited to 120 square feet in size. Projecting signs are limited to 48 square feet in size. The height limitation for both wall signs and projecting signs is 34 feet. Freestanding monument signs are limited to 32 square feet in size and can be no taller than 8 feet. However, a freestanding monument sign shall not be allowed if the amount of signage exceeds two-and-a-half square feet of signage for every one foot of primary building wall. The zoning code limits the number of freestanding signs on a zoning lot to one.
- The applicant is showing placeholders for a total of nine wall and/or projecting signs. From the information provided, all of the signs meet the dimensional requirements of the zoning code.

Screening of Mechanical Equipment – Meets requirements

- All mechanical equipment is subject to the screening requirements of Chapter 535, Regulations of General Applicability.
- All mechanical equipment will be enclosed within the building on the ground level or on top of the building.

Refuse Screening – *Meets requirements*

- All refuse and recycling storage containers are located within the building.

Lighting – *Meets requirements with Conditions of Approval*

- Existing and proposed lighting must comply with Chapter 535, Regulations of General Applicability.
- A lighting plan showing footcandles was not submitted as part of the application materials. CPED is recommending that the final lighting plan conform to the standards of Chapter 535, Regulations of General Applicability.

Fences – *Meets requirements*

- Fences must comply with the requirements in Chapter 535, Regulations of General Applicability.
- The applicant is proposing a five-foot tall decorative metal fence around the perimeter of the pet exercise area. The fence meets the requirements of Chapter 535, Regulations of General Applicability.

Specific Development Standards – *Not applicable*

- The proposed use is not subject to specific development standards in Chapter 536, Specific Development Standards.

DP Downtown Parking Overlay District Standards – *Meets requirements*

- The proposal is in compliance with the standards of the DP Downtown Parking Overlay District.

Applicable Policies of the Comprehensive Plan

The Minneapolis Plan for Sustainable Growth identifies the site as mixed use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.1: Promote building designs and heights that enhance and complement the image and form of the Downtown skyline, provide transition to the edges of Downtown and protect the scale and quality in areas of distinctive physical or historical character.

- 10.1.1 Concentrate the tallest buildings in the Downtown core.
- 10.1.2 Building placement should preserve and enhance public view corridors that focus attention on natural or built features, such as landmark buildings, significant open spaces or water bodies.
- 10.1.3 Building placement should allow light and air into the site and surrounding properties.

Urban Design Policy 10.2: Integrate pedestrian scale design features into Downtown site and building designs and infrastructure improvements.

- 10.2.1 The ground floor of buildings should be occupied by active uses with direct connections to the sidewalk.
- 10.2.2 The street level of buildings should have windows to allow for clear views into and out of the building.
- 10.2.3 Ensure that buildings incorporate design elements that eliminate long stretches of blank, inactive building walls such as windows, green walls, architectural details, and murals.
- 10.2.4 Integrate components in building designs that offer protection to pedestrians, such as awnings and canopies, as a means to encourage pedestrian activity along the street.
- 10.2.5 Locate access to and egress from parking ramps mid-block and at right angles to minimize disruptions to pedestrian flow at the street level.

- 10.2.6 Arrange buildings within a site in order to minimize the generation of wind currents at ground level.
- 10.2.7 Locate buildings so that shadowing on public spaces and adjacent properties is minimized.
- 10.2.8 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

CPED finds that the proposed development is in conformance with the above policies of *The Minneapolis Plan for Sustainable Growth*.

Applicable Development Plans or Objectives Adopted by the City Council

The site is located within the boundaries of the Historic Mills District Master Plan (1998), the Update to the Historic Mills District Plan (2001) and the Downtown East/North Loop Master Plan (2003). The Historic Mills District Master Plan and the Update to the Historic Mills District Plan assume that the former Star Tribune building that was on the site remained. However, if redeveloped, the plans call for eight to nine stories of residential. The Downtown East/North Loop Master Plan identifies the site as medium-density (five to 13 floors), mixed use with residential. The site is located in the Washington Village Development Precinct. The plan says:

The area immediately north and west of the new Downtown East LRT Station is a promising candidate for a new TOD neighborhood. A collection of underdeveloped properties – many of which are existing surface parking lots – are located within easy walking distance of the station site. Many of these blocks are excellent sites for full-block, half-block, quarter-block and infill development projects. This area provides the best opportunity to create a new “Complete Community” that would integrate existing structures and uses with new development. Creating a medium-intensity, mixed use district in this precinct would add a major residential component to Downtown East; one that is immediately adjacent to the Downtown Core and within easy reach of the amenities located in the Mills District and along the Central Riverfront.

This precinct should be focused on Chicago Avenue, which would serve as a pedestrian-friendly link between two retail concentrations, one at the LRT station and another along Washington Avenue. The City of Minneapolis has already expressed a desire to create a strong TOD node at the Downtown East station by forging a mixed-use project that will integrate a new commercial office building, an outdoor neighborhood plaza, and at-grade convenience retailing all within the same block as the new LRT station. The north half of the block between Park Avenue, Chicago Avenue, South 5th Street, and South 6th Street should be further developed to include street-level retail that would help create an identifiable retail / transit node at the Downtown East LRT station.

While taller than envisioned, this development is meeting the objectives of these small area plans.

Alternative Compliance

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building placement.** The first floor of the building is located between three and ten feet from the front lot line along South 4th Street. Where the building is set back more than eight feet is where the principal entrance to the building is located. The entrance has been recessed further into the building wall in order to provide for a deeper canopy for weather protection. CPED is recommending that the City Planning Commission grant alternative compliance.

- **Ground floor active functions.** The ground floor facing South 4th Street contains 78 percent (173 feet) active functions and the ground floor facing Park Avenue contains 68 percent (48 feet) active functions. The total length of the building wall facing Park Avenue is 71 feet. The inactive portion of the building wall is 23 feet in length; the length of the garage door. CPED is recommending that the City Planning Commission grant alternative compliance.

PRELIMINARY/FINAL REGISTERED LAND SURVEY

The Department of Community Planning and Economic Development has analyzed the application for a preliminary and final registered land survey based on the following findings:

1. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

The existing lot will be subdivided into two tracts; one for the MSFA parking garage and one for the proposed residential tower that includes air rights above a majority of the existing parking garage. Individual lots within a planned unit development are exempt from the public street frontage requirement of section 598.230 and the design requirements of sections 598.240 and 598.250. The subdivision is in conformance with the design requirements of the land subdivision regulations.

Section 598.260 Planned unit developments and cluster design, requires the design of a subdivision for a planned unit development to implement the site plan as approved by the Planning Commission and shall include a deed restriction designating the following:

- a. *The relationship between all common spaces and each individual lot (rights in the common spaces and proportionate ownership accruing to the individual lot).*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- b. *Provision for access to each lot that does not have frontage on a public street.*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- c. *A requirement that an owner's association be created. The duties and responsibilities of the owner's association shall include maintaining the elements of the planned unit development as authorized under the zoning ordinance or other applicable regulations.*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- d. *A provision that the taxes, special assessments, and other charges and fees that would normally be levied against the common spaces shall be levied against the individual lot occupied or to be occupied by buildings in direct proportion to the interest that is stated in the deed restriction and shall provide that such levies shall be a lien against the individual lots.*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- e. *A requirement that any disposition of any of the common property situated within the planned unit development shall not be made without the prior approval of the Planning Commission.*

If an amendment is made to the Planned Unit Development the proposed amendments shall be reviewed and approved by the City Planning Commission.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

The land intended for building can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

The tracts created by this application present no foreseeable difficulties for this development. No significant alterations to the land appear necessary.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Ryan Companies US, Inc. for the property located at 740 South 4th Street:

A. Conditional Use Permit for a Planned Unit Development.

Recommended motion: **Approve** the conditional use permit to allow for Phase V of a Planned Unit Development including a 342-unit residential building, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

2. As required by section 527.120 of the zoning code, the development shall comply with the standards for some combination of the following amenities from Table 527-1, Amenities and those proposed by the applicant totaling a minimum of five points: shared vehicles, recycling storage area and pet exercise area.
3. No less than three shared vehicles shall be provided on the site.

B. Site Plan Review.

Recommended motion: **Approve** the site plan review for a 342-unit residential building, subject to the following conditions:

1. All site improvements shall be completed by March 11, 2021, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. The final lighting plan shall conform to the standards of Chapter 535, Regulations of General Applicability.

C. Preliminary/Final Registered Land Survey.

Recommended motion: **Approve** the application for a Registered Land Survey.

ATTACHMENTS

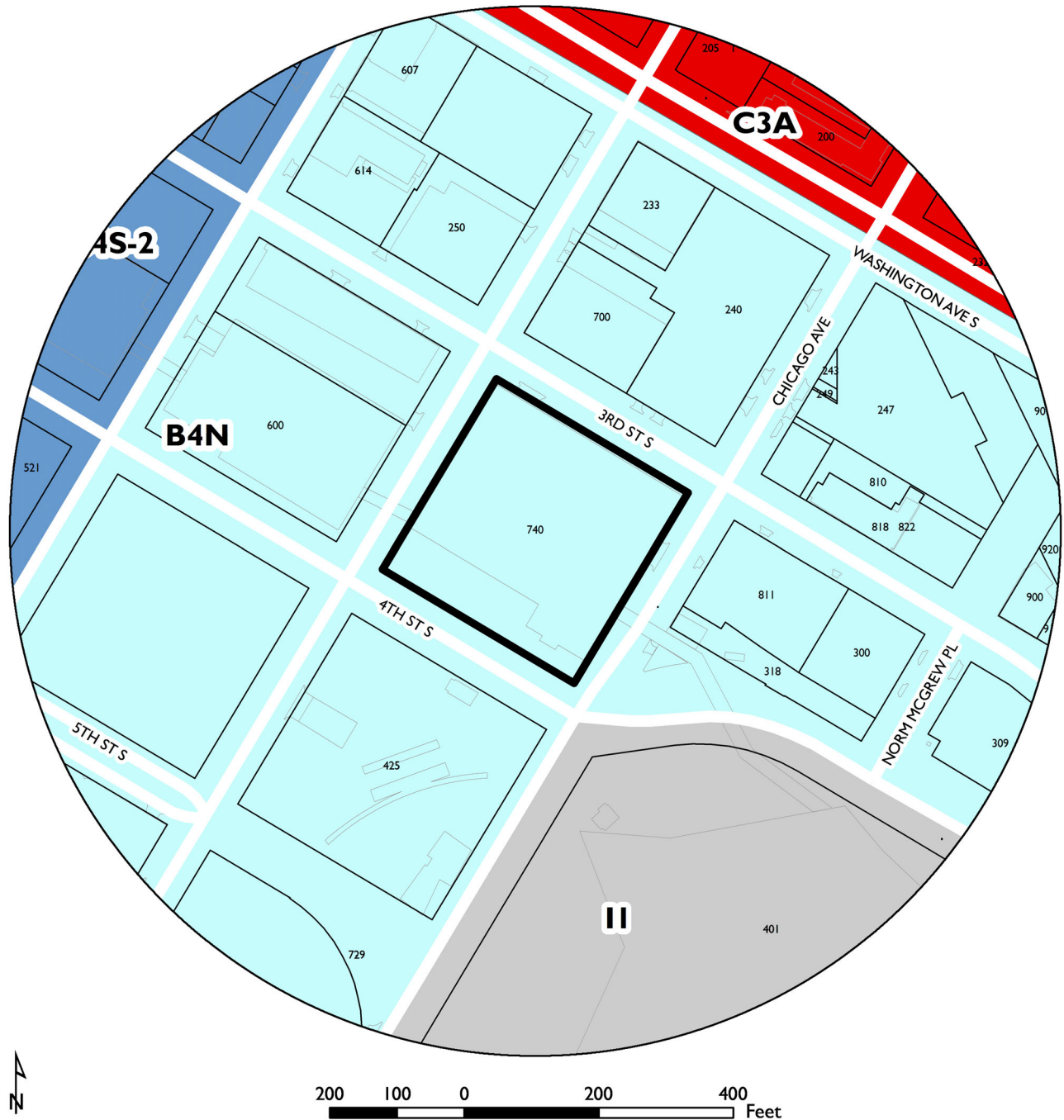
1. Zoning map
2. PDR report
3. Written description and findings submitted by applicant
4. RLS
5. Aerial and site photos
6. Shadow study and context study
7. Renderings
8. Civil plans
9. Landscape plans
10. Architectural plans
11. Correspondence

Ryan Companies US, Inc.

3rd

NAME OF APPLICANT

WARD



PROPERTY ADDRESS

740 South 4th Street

FILE NUMBER

PLAN8182

PDR Comprehensive Report

Project Name	345 Unit 25 Story Apt	
Site Address	740 4TH ST S	
PDR Meeting Date	12/12/2018	
Development Coordinator Assigned:	JENNIFER THORESON (612)673-5867 jennifer.thoreson@minneapolismn.gov	
Status	PDR Number:	PLAN7911
Resubmission Required	Applicant	RYAN CO US INC 533 3rd St S #100 Minneapolis, MN 55415

DISCLAIMER: The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification. Because of the nature of the process, resubmittals may result in additional or modified comments; note however, that efforts will be made to limit additional comments to only those areas that are subject to revision.

You are required to resubmit a complete electronic copy of the updated plan for review by City staff to ensure that all comments below have been incorporated. The project cannot move forward to Formal Site Plan approvals and permitting until the PDR process is complete.

In addition to the revised site plan, please provide a written response to all comments (a Comment Response Sheet), which at a minimum, should include identification of the commenting City Department followed by the corresponding response and site plan page references. **The comment response sheet should be labeled and uploaded to the documents folder.**

As a general rule:

1. All comments shall be addressed.
2. Provide explanations as to how a particular comment was addressed; or
3. Why a comment was addressed differently than requested; or
4. Why a particular comment or request could not be accomplished.

Please submit your plans via the city e-planning online portal. You should have received an email at {enricow@kaaswilson.com} with a link. Please provide a complete set of updated plans (to include site, civil, landscaping, elevation, floor plans, and all associated documents identified below) for review by City staff to ensure that all comments have been incorporated. The project cannot move forward to Formal Site Plan approvals and permitting until the PDR process is completed.

Review Findings (by Discipline)

Address Review Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.

- The address for the proposed Block 1 residential building will be **710 4th Street South**. This address meets the City of Minneapolis Street Naming and Address Standard requirements.

When assigning suite sequences the following guidelines are as follows:

- The first one to two digits of the suite sequence number will designate the floor number of the site.
- The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
- Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
- Please provide each condo, suite, unit or apartment number.

Business Licensing Review

There is not a Minneapolis Business Licensing review required for the proposed project.

Construction Review Contact **Scott Anderson at 612-673-2631** or C.Scott.Anderson@minneapolismn.gov with questions

- Accessible parking is required in each parking facility. No accessible parking stalls currently shown.
- Clarify if the new structure is intended to be a separate building or if it is an addition to the existing structure.
- Issues relating to fire separation distance and opening protection need to be addressed.
- Structure exceeds 120 feet in height so fire service access elevator requirements need to be addressed.
- A Service Availability Charge (SAC) determination will need to be submitted to the Metropolitan Council for the proposed project. Please refer to this link for more information [SACprogram@metc.state.mn.us](http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf). or http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf

Environmental Review Contact **Tom Frame at 612-673-5807** or Tom.Frame@minneapolismn.gov with questions

- Determine the average and higher ground water mark and use that as a basis for the lowest levels of subgrade structures including underground parking.
- A permit is required to dewater to City Sewers and Drains during construction. Permanent dewatering to maintain subgrade space is not acceptable.

Fire Safety Review

- Provide automatic fire suppression and fire detection systems throughout building per building and fire codes
- Fire department connection must be on the address side of building and within 150 feet of a fire hydrant
- Provide and maintain fire apparatus access at all times

Forestry Contact **Craig Pinkalla (612-499-9233 cpinkalla@minneapolisarks.org)** regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way.

- Proposed boulevard tree removal requires \$400.00 compensation payable to MPRB prior to removal
- Remaining boulevard trees to be identified for protection per city detail FORE- 1001
- Landscape plan does not include tree and open planting space on east side of driveway entrance.

Park Board Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:

<http://library.municode.com/index.aspx?clientId=11490>

As adopted, the fee in lieu of dedication for new residential units is \$1576.20 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$210.00 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.

There is also an administration fee that is 5% of the calculated park dedication fee.

As proposed, for your project, the calculated dedication fee is as follows:

Park Dedication Fee Calculation =

- Residential (345 units x \$1576.20 per unit) = \$543,789.00
- Non-Residential Commercial Space = \$N/A
- 5% (Administration Fee max \$1,000.00) = \$1,000.00
- **Total Park Dedication Fee: \$544,789.00**

This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal. For further information, please contact **Jennifer Thoreson at 612-673-5867**.

Zoning Review Contact **Hilary Dvorak at 612-673-2639** or Hilary.Dvorak@minneapolismn.gov with questions

The conditions of approval will need to be incorporated into the plans that are resubmitted for PDR.

Ramp

This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

Right of Way Review

- **Note to the Applicant:** the City of Minneapolis will be hosting the NCAA Final Four in April of 2019. Although exact details are not available at this time, it should be assumed that a moratorium on all construction activity impacting the Public right-of-way, and issuance of street use and obstruction permits will be in place for multiple weeks leading up to the event.
- An encroachment permit shall be required for all non-standard streetscape elements in the Public right-of-way such as: shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact **Matt Hanan at (612) 673-3607** for further information.
- **Note to the Applicant:** Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact **Matt Hanan at (612) 673-3607** for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- The site plan does not indicate locations of proposed electric utility transformers; it is recommended that the Applicant begin discussions with Xcel Energy as soon as possible in order to identify electric utility and transformer locations on the site plan. To the extent possible, transformers should be located on private property; it should not be assumed that the City will approve any proposed transformer location in the Public right-of-way.
- **Note to the Applicant:** Per the Minneapolis Code of Ordinances a sub-surface transformer vault is defined as an "Areaway" and must meet the City's design standards for an Areaway and will require an encroachment permit. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road_. The Applicant shall be responsible for all costs of design and construction necessary to insure any proposed areaway meets the City's design standards.
- The Project limits fall within the boundaries of the Downtown Improvement District (DID). Any improvements, modifications, and alterations to the streetscape are subject to the review and approval of the DID. Please contact **Ben Shardlow at BShardlow@mplsdid.com** for further information.

Sewer Review

Groundwater:

- Identify if any pumping of groundwater is proposed in order to keep the below grade areas dry after completion of construction. If not, this should be noted on the plans.
- Any proposed groundwater discharges require review by the City. Please also include the low floor elevation on the Grading Plan.

Stormwater Management:

- Provide the supporting documentation for the stormwater management modeling, such as drainage area maps, existing conditions stormwater modeling, plans & O&M for stormwater management BMPs constructed on adjacent sites that appear to be proposed to be utilized to meet requirements, etc.
- If a shared private stormwater management BMP is proposed, private easements and/or agreements will need to be entered into among the properties detailing this condition and, ownership and maintenance responsibilities. These easement/agreement documents will need to be recorded with the County. Provide copies of the documents to the City.

Sidewalk Review

The plan as submitted meets the requirements of the Public Works Sidewalk Inspections Division.

Street Design Review

The plan as submitted meets the requirements of the Public Works Street Design Division.

Traffic and Parking Review

- **Note to the Applicant:** the City of Minneapolis will be hosting the NCAA Final Four in April of 2019. Although exact details are not available at this time, it should be assumed that a moratorium on all construction activity impacting the Public right-of-way, and issuance of street use and obstruction permits will be in place for multiple weeks leading up to the event.
- The Applicant shall note the location of any existing Metro Transit “bus stops” on the site plan. **Note to the Applicant:** Please coordinate any needed short-term lane closures with Metro Transit Street Operations, **Demetairs Bell (612-349-7381 / demetairs.bell@metrotransit.org)**
- Current ordinance states that all maneuvers associated with vehicle circulation, loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all vehicle types that will be using the loading dock/parking entrance areas. Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- Ramp slopes for all vehicle access points shall be less than a 4 percent slope for the first 20' from the property line (please indicate slope on the drawings). If these minimums cannot be attained the Applicant shall provide mitigating measures, such as pedestrian warning devices, signage, and slip prevention measures on the ramp.
- Generally, a vehicle should not be stopped on the sidewalk or in a moving lane of traffic waiting for a garage door to open. Please provide a narrative explaining garage door operation on both Park Ave. S. and 4th St. S. Vehicle access points should have 20' of stacking room between the property line and any garage door. The vehicle access point on both Park Ave. S. and 4th St. S. shall provide for an unobstructed 20' sight triangle. If these minimums cannot be attained the Applicant shall provide mitigating measures, such as pedestrian warning devices, signage, and slip prevention measures on the ramp.
- Minneapolis Traffic has underground infrastructure within the project boundaries that are not shown on the site plan. All Traffic infrastructure shall be clearly shown on the site plan. Please contact **Darryn Proch at (612) 673-5516** or at darryn.proch@minneapolismn.gov for further information.
- All street lighting (existing and proposed) shall be shown clearly on the site plan. If City owned street lighting exists on the proposed site it must be preserved or replaced at existing levels. All project work areas must be surveyed prior to construction for the presence of street lights, traffic signals, cabinets and signal interconnect conduit. If the project requires the temporary removal/temporary relocation of any City of Minneapolis lighting or traffic signal system that may be in the way of construction, or will disrupt the sidewalks and excavate to a point where signals could be damaged or disrupted, the project must budget for relocation or full replacement of street lights, signal interconnect and signal foundations and conduit.
- **Note to the Applicant:** All costs for relocation and/or repair of City Traffic facilities including traffic signal systems, street lighting, traffic signs, parking meters, and pavement markings shall be borne by the Contractor and/or Property Owner. In addition to the various required construction permits, impacts to existing traffic signal and street lighting systems (including installation of new street lights) will require the Applicant and respective Contractors to enter into a separate Right-of-Way Excavation Permit (including Testing and Inspection requirements) with the Public Works Department, for further information regarding this permit please coordinate with **Tilahun Hailu at (612) 673-5809**.
- **Note to the Applicant:** Street lighting and/or traffic signal systems installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact **Dave Prehall at (612) 673-5759** for further information. Any lighting or traffic signal system installations not meeting City specifications will be required to be reinstalled at Owner expense. The City of Minneapolis Traffic Department is available to install street lighting systems by agreement and will provide an estimate of installation costs as requested, please contact **Joe Laurin at (612) 673-5987** for further information.

Water Review

- The City of Minneapolis Water Treatment & Distribution Services Division (WTDS) requires that potable water supply service lines shall be sized based upon the total demand and shall be determined in accordance with recognized engineering methods and procedures. The Applicant (and Engineer) shall be responsible for designing potable water systems that are not oversized for their intended use so that turnover is sufficient to maintain water quality. Domestic water and fire service connections should run in a line perpendicular from the watermain straight into the proposed building to the meter location. Meters shall be set at the point where the service line enters the building and shall be set in a location where it is easily accessible. Please contact **Rock Rogers at (612) 673-2286**, to confirm domestic and fire service design, connections, and sizes. The Applicant shall provide a confirmation of domestic and fire service design methods before site plan approval can be granted.

Resubmittal instructions for ProjectDox (you should be using Internet Explorer 11)

1. **Name your revised files** the same name that you used for the original submittal **and save** them individually on your computer
2. Log into Project Dox (Using Internet Explorer 11)
3. Click on the **project number** that was included in your revision email
4. Click on the **drawings folder**
5. Click the **upload Files** box (**DO NOT delete** the files that were previously uploaded!!!!)
6. Upload you revised plans (named the same as your originals) by **browsing or dragging**
7. Click **Upload Files**
8. Click **Close window**
9. Complete the same tasks above to upload any new/revised documents in the **documents folder**. **You must upload your comment response sheet to this (document) folder.**
10. Click the **Workflow Portals** button
11. Click on the **Applicant Resubmit**
12. Do you want to accept? **Click OK**
13. Under **Task Instruction**, read and check the three boxes (if you experience difficulties or cannot see these boxes you should confirm that you are using Internet Explorer 11 and the ProjectDox Components have been installed on the computer you are using)
14. Click the **Complete box** at the very end of the page
15. If everything is complete/submitted, you will have any task available (to complete)



December 20, 2018



Hilary Dvorak
Principal City Planner
City of Minneapolis – Community Planning and Economic Development (CPED)
250 South Fourth Street, Room 300
Minneapolis, Minnesota 55415
612-673-2639

**RE: Block 1 Residential
 710 4th Street South
 Land Use Application(s) Submittal**

Dear Ms. Dvorak:

Ryan Companies US, Inc. is pleased to submit the following Land Use Applications for the Block 1 Residential project located at 710 4th Street South (PID 2602924210171):

- Conditional Use Permit for Phase V of a Planned Unit Development
- Site Plan Review
- Subdivision

The following project description, project data and proposed PUD site amenities are summarized below.

Project Description

The proposed project generally consists of subdividing the remaining Downtown East PUD development parcel on the southwest corner of the lot occupied by the MSFA parking ramp, along with the air rights over the ramp, and constructing a 342-unit 25-story apartment tower with 120 structured parking stalls. One level of subgrade parking will be accessed from an existing curb cut on Park Avenue. Some additional parking and room for truck loading and maneuvers will be located on the ground level, accessed from a curb cut on 4th Street. This parking and loading area will be tucked behind some active lobby, amenity and/or commercial spaces. The

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533 South Third Street, Suite 100
Minneapolis, MN 55415

p: 612-492-4000
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remainder of the parking will be located on floors 3 through 5 and be accessed through the existing parking ramp.

For a more extensive project description and design, please see the attached exhibits and plans.

Project Data

- Existing Zoning = B4N/PUD
- Existing Overlay Districts = DP
- Site Area
 - Existing = 109,411 SF
 - Proposed

Tract A	18,226 SF
Tract B	91,185 SF
<u>Total</u>	<u>109,411 SF</u>
 - See attached VRLS for specifics on how the air rights are subdivided.
- Gross Floor Area (GFA) = 319,475 SF
- Building Height = 25 stories (24 occupiable), 305 ft
- Residential Dwelling Units = 342
- Commercial GFA = 1,070 SF
- Off-Street Parking

Standard Spaces	22 (21 part of tandem)
Compact Spaces	93 (45 part of tandem)
<u>Accessible Spaces</u>	<u>5 (1 van accessible)</u>
<u>Total</u>	<u>120</u>
- Bicycle Parking = 250 spaces
- Loading = 2 small

Proposed PUD Site Amenities

- Shared vehicles (3)
- Recycling storage area
- Proposed amenity – pet exercise area with different dimensions

Please do not hesitate to contact me with any questions regarding this submittal or if additional information is needed.

Sincerely,

Tom Rehwaldt, PE
Civil Engineer
Ryan Companies US, Inc.
612-492-4712
thomas.rehwaldt@ryancompanies.com



December 19, 2018



**RE: Block 1 Residential
 Required Findings**

Ryan Companies US, Inc. is providing statements addressing the following required findings to support the proposed conditional use for Phase V of the Downtown East planned unit development that includes an increase in maximum building height and subdivision:

Conditional Use Permit

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

This phase of the PUD will not be detrimental to or endanger the public health, safety, comfort or general welfare. The proposed development is in line with the vision for this parcel established in the original PUD back in 2013. A well-lit ADA compliant pedestrian pathway of at least 6 feet in width will be maintained along 4th Street and Park Avenue. Large glass facades looking out from active uses along the first level will add security to the sidewalk and Commons across the street.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

This property is the final development parcel of the Downtown East PUD. The intent has been to develop this parcel with a building of the scale and quality being proposed. The addition of this high quality multi-family tower will add additional residents to the Downtown East area and improve the desirability of the neighborhood.

The proposed design improves some of the top-level parking spaces of the adjacent garage by constructing an amenity deck over the southernmost parking bay. This addition will provide shelter for parking stalls that are currently exposed to the elements.

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3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

All proposed utilities, drainage and access elements have been designed adequately for this project. The adjacent MSFA parking ramp was constructed with a stormwater management system that has capacity to treat this parcel with 100% impervious coverage. The Public Works Department will review plans for compliance with standards related to access and circulation, drainage and sewer/water connections. The proposed project will be permitted by the City of Minneapolis prior to construction commencement.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

A traffic study was completed for this project as part of the AUAR for the 5 block Downtown East development. The study found that the future traffic with the Maximum Development Scenario would be acceptable and that all intersections would still operate within a reasonable level of service. With this project completing the Downtown East development, the entire study area is within the constraints of the Maximum Development Scenario.

The location of the site is ideal when it comes to the availability of alternative modes of transportation. A light rail station providing access to the Blue and Green lines is within one block. Several bus lines are accessible from stops surrounding the area. The site is also a short walk or bike ride away from hundreds of restaurants, businesses and services located Downtown or in the Mill District. Raised bike paths were constructed as a part of Phase 1 of the Downtown East development, creating a safer environment for bike transportation.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

This proposed phase of the PUD is consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

1.1.1 Ensure that the City's zoning code is consistent with The Minneapolis Plan and provides clear, understandable guidance that can readily be administered.

1.1.3 Encourage the use of flexible regulatory options that promote high quality development, such as the Planned Unit Development (PUD) tool.

1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features;

minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

1.1.7 Invest in targeted place-making strategies to build upon and enhance existing community assets and encourage private sector development.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy 1.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

1.13.4 Encourage architectural design, building massing and site plans to create or improve public and semi-public spaces near the station.

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

1.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

Housing Policy 3.1: Grow by increasing the supply of housing.

3.1.1 Support the development of new medium- and high-density housing in appropriate locations throughout the city.

Housing Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

10.5.3 Large-scale, high-rise, multi-family residential development is more appropriate in the Downtown Minneapolis growth center.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposed project will comply with the provisions of the B4N Downtown Neighborhood District and the DP Downtown Parking Overlay District.

Increasing Maximum Height

1. *Access to light and air of surrounding properties.*

Located on the southern edge of the parking garage, this project does not impact access to light and air for buildings that are occupied during the day. The LRT Plaza and The Commons are located to the south of the property and will be unaffected by the shadows of the new reinforced urban edge to the north.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The proposed building is of a similar scale to the Wells Fargo buildings to the west and will not add significant shadowing to surrounding residential uses. The recently completed East End apartments will have some minimal impact during winter months in the late afternoon.

3. *The scale and character of surrounding uses.*

The proposed project is in line with the scale and character of surrounding uses. Specifically, the Wells Fargo buildings nearby are of a very similar height and overall presence, and the scale of US Bank Stadium to the east is itself out of scale with anything around it. From a use standpoint, this is an extension of the residential use that already surrounds The Commons, just at a higher density.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

Thresher square is the nearest historically significant building near the site, and it is unimpacted by this project. US Bank Stadium is a culturally significant building immediately adjacent to the site, but views are not blocked from any public uses by this project.

Planned Unit Development

1. *The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.*

This project consists of primarily residential use with ancillary retail on the ground floor. Relative to nearby properties, it presents a new opportunity to live in a high-rise with dramatic views in all directions. A variety of housing types are proposed, including micros/studios/alcoves, 1 bedrooms and 2 bedrooms.

2. *The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provision of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

A traffic study was completed for this project as part of the AUAR for the 5 block Downtown East development. The study found that the future traffic with the Maximum Development Scenario would be acceptable and that all intersections would still operate within a reasonable level of service. With this project completing the Downtown East development, the entire study area is within the constraints of the Maximum Development Scenario.

Our current proposal includes approximately 120 new structured parking stalls accessed from either Park Avenue, 4th Street or the adjacent MSFA parking ramp. Tenants will utilize alternative modes of transportation based on the numerous options in the area and the close proximity to businesses and services. Residents will also be able to utilize the adjacent ±1,600 stall parking ramp.

Loading areas will be located on Level 1 accessed from 4th Street. Also, the building will have exceptional bicycle amenities that exceeds the code-required number of bike parking in dedicated bike storage rooms.

3. *The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

The proposed site amenities include 3 shared vehicles, a recycling storage area, and an amenity proposed by the applicant (pet exercise area). Additional open space amenities that can be utilized by tenants of this building will be the amenity deck that will be constructed above the top of the adjacent parking ramp and The Commons park across 4th Street.

4. *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development, and protection of views and corridors.*

- Building scale – the building scale is very compatible with neighboring and nearby uses.

- Massing – the massing reinforces the urban wall along 4th street bordering The Commons, and improves the unfinished façade of the Mills Fleet Farm Garage.
 - Microclimate effects – since the building fronts the south side of a parking garage, microclimate effects are minimal. The top floor of the parking garage will have more shadow on it and likely have slightly cooler temperatures year-round than it currently does.
 - Protection of views and corridors – if anything, this building improves views in the area by completing the urban wall started by the Wells Fargo buildings and US Bank stadium. It adds to the downtown skyline from the east and north as well.
5. *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing, and applicable policies of the comprehensive plan and adopted small area plans.*

The closest residential use is located approximately 90 feet away from the proposed building, across Park Avenue. Save for a few windows on the southeast corner units, the majority of the units face the Commons park, so the majority of views will go unchanged.

6. *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

The adjacent MSFA parking ramp was constructed with a stormwater management system that has capacity to treat this parcel with 100% impervious coverage. All roof drains from the proposed project will tie into an existing stub on the southwest corner of the ramp, which will route the stormwater to the filtration system beneath the northeast corner of the ramp. All temporary and permanent erosion and sediment control measures will comply with all City, State and Federal requirements.

7. *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

Sustainable building practices during construction and demolition phases will be incorporated. Solid waste providers will be selected that utilize off-site recycling centers.

Subdivision

1. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

The subdivision is in conformance with the applicable zoning ordinance regulations, and general land use, housing, economic development, environment, open space and parks, and urban design policies of the comprehensive plan.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

The existing lot will be subdivided into two tracts; one for the MSFA ramp and one for the residential tower that includes air rights above a majority of the existing ramp. This will have no negative effects on the surrounding community. The traffic study within the AUAR confirms that the traffic congestion on the public streets will be acceptable without mitigation measures.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

There are no flooding concerns for the site or surrounding area (located in Zone X). Erosion and sedimentation control measures (temporary and permanent) will meet all applicable requirements. Groundwater was encountered as part of the geotechnical site investigation at depths of 39 to 41.5 feet below existing grade. Proper site drainage will be implemented meeting City requirements. Steep slopes do not exist and are not proposed as part of the development. Bedrock was encountered as part of the geotechnical site investigation at depths of 36 to 44 feet below existing grade. All soil contamination that existed on-site was remediated per an approved Response Action Plan when the parking ramp was constructed.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

There are no foreseeable issues with building our development given the proposed lot arrangement. The three access points for the MSFA ramp will remain perpendicular to their property. The new residential lot will have street frontage along both Park Avenue and 4th Street, with curb cut access proposed along both. Entrances to the new proposed structured parking on levels 3 through 5 will be provided through the existing ramp. Discussions with MSFA about easements to allow this are underway. The amenity deck over the southern parking bay of the existing parking ramp will be located within the air rights granted to our development.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The existing site is currently grassy open space that sheet flows to the City storm system in Park Avenue and 4th Street. Once fully developed, all of this stormwater will be piped to the filtration system beneath the MSFA ramp, where it will be treated before being piped to the City storm system. The outlet of the system has been designed so that runoff rates do not



exceed the runoff rates prior to the Downtown East development for the 2-year, 10-year and 100-year storm events. All temporary and permanent erosion and sediment control measures will comply with all City, State and Federal requirements.



December 19, 2018



**RE: Block 1 Residential
PUD Site Amenity Summary**

Ryan Companies US, Inc. is providing the following summary of proposed site amenities to support the proposed phase of the planned unit development.

Required Amenity Points

Amenity points required for the requested authorized alternatives:

Authorized Alternatives	Points
Bulk regulations	
To increase maximum height of structures	5
Off-street parking and loading	
Minimum amount of required off-street parking	0
Minimum width of parking aisles	0
Amenity	
Planned unit development (phase 5 of ex. PUD)	0
Total	5

- **Increasing the maximum height of structures:** In the B4N Downtown Neighborhood District, the maximum height of principal structures is 10 stories or 140 feet, whichever is less. The proposed building is 25 stories and 305 feet tall.
- **Reducing the minimum amount of required off-street parking:** In Downtown districts, the only minimum parking requirement for residential uses is that for multiple-family dwellings of 50 or more units that provide off-street parking for residents, one parking space per 50 dwelling units will be designated for guest parking. We would like to remove this requirement for a number of reasons. We would like to have the flexibility to offer the parking stalls that we are able to fit on this small site to the tenants, whether it be residential or commercial. Also, there is plenty of opportunity for guests to utilize the attached Mills Fleet Farm parking ramp for their temporary parking needs.
- **Reducing the minimum width of parking aisles:** Per Minneapolis code, parking aisles need to be 22 feet wide. We are requesting approval to incorporate drive aisles with a

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Minneapolis, MN 55415

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minimum width of 18 feet. Based on the minimal amount of parking stalls and vehicle traffic, we feel this is adequate.

Proposed Amenity Points

The project proposes the following **5 points** of site amenities, as defined in Table 527-1 Amenities, of the Planned Unit Development Application:

- **Shared Vehicles (3 points)**

- Standards:

- Access to a shared passenger automobile available for short-term use. For residential uses, a minimum of one (1) car per one hundred (100) dwelling units is required.

- Proposed Design:

- Per guidance provided in our Committee of the Whole staff report, we will be required to provide three shared vehicles to receive these points based on the number of units we are proposing (342).

- **Recycling storage area (1 point)**

- Standards:

- Provide an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building.

- Proposed Design:

- The proposed building will have easily accessible dedicated rooms for trash and recycling storage that serve the entire building and will be entirely enclosed within the parking podium. The rooms will be dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals.

- **Amenities proposed by the applicant or others (1 point)**

- Standards:

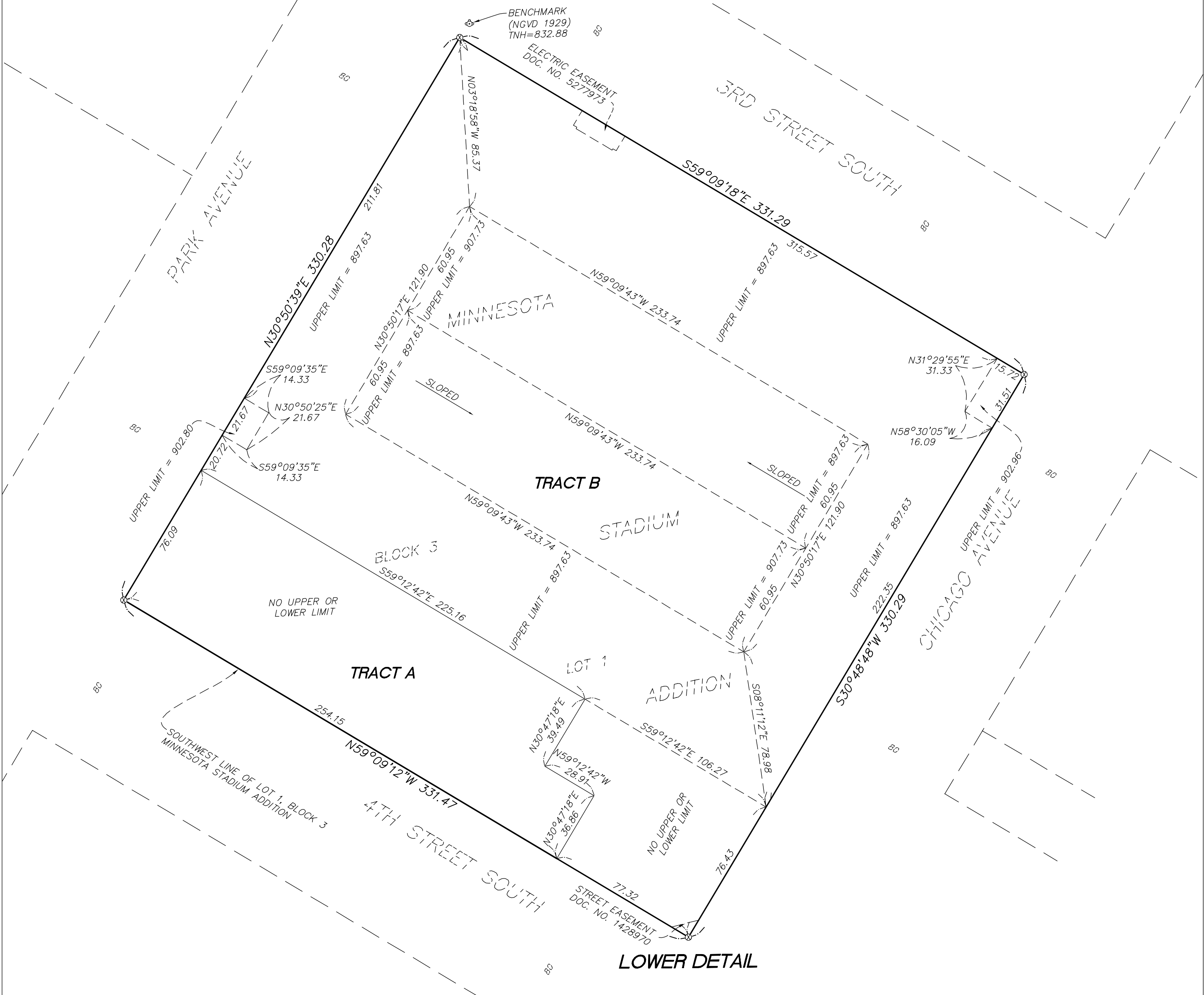
- The city planning commissions may consider other amenities not listed in Table 527-1, Amenities, that are proportionally related to the alternative requested. The commission may assign one (1), three (3), five (5), or ten (10) points based on the proportionality.

- Proposed Design:

- We are proposing that our pet exercise area receive 1 amenity point in this category because it doesn't meet the exact standards listed for a pet exercise area in Table 527-1, but we believe it meets the intent. The standard for the pet exercise area amenity point is that the area be at least 12 feet by 60 feet. This equates to an area of 720 square feet. The pet exercise area we are proposing, located at grade on the southeast corner of the site, has an area of approximately 790 square feet. Our pet exercise area will meet all other requirements listed in Table 527-1, including being enclosed with decorative fencing, having adequate lighting and providing accommodations for proper disposal of animal waste.

REGISTERED LAND SURVEY NO.

R.T. DOC. NO.



I, Nathan H. Carlson, hereby certify that, in accordance with the provisions of Minnesota Statutes, Section 508.47, I have surveyed the following described property situated in the County of Hennepin, State of Minnesota.

Lot 1, Block 3, MINNESOTA STADIUM ADDITION.

Torrens Property
Certificate of Title No. 1425601

I hereby certify that this REGISTERED LAND SURVEY was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota and that this REGISTERED LAND SURVEY is a correct representation of said parcel of land. Dated this ____ day of _____, 20____.

Nathan H. Carlson, Land Surveyor
Minnesota License No. 45873

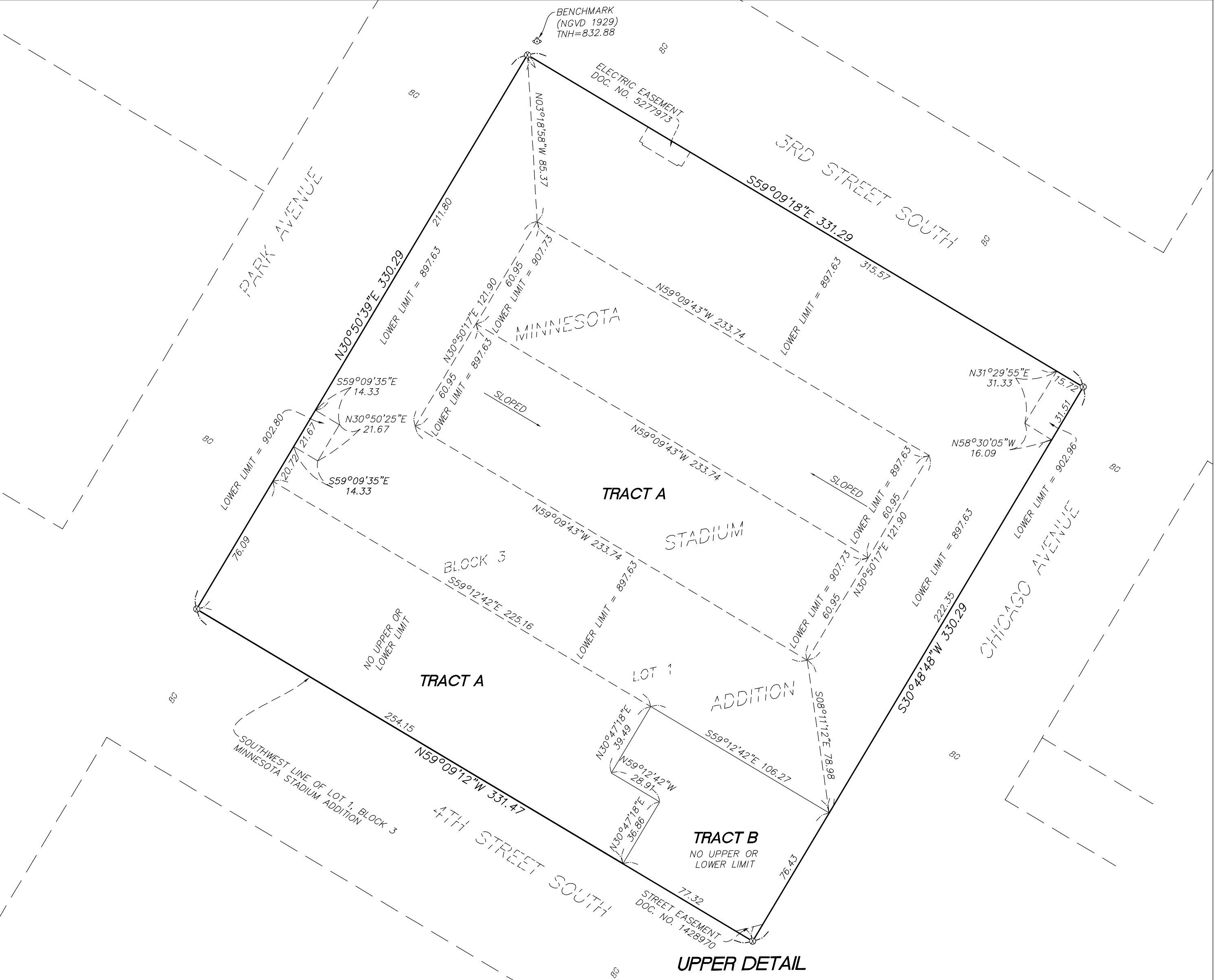
STATE OF MINNESOTA
COUNTY OF _____
The foregoing Surveyor's Certificate was acknowledged before me this ____ day of _____, 20__, by Nathan H. Carlson, Land Surveyor, Minnesota License No. 45873.

(Signature) _____ (Name Printed) _____

Notary Public, _____ County, Minnesota
My Commission Expires _____

PLANNING COMMISSION, MINNEAPOLIS, MINNESOTA
I, The Secretary of the Planning Commission of the City of Minneapolis, Minnesota, do hereby certify that on this ____ day of _____, 20____, the City of Minneapolis acting by and through its City Planning Commission duly approved this REGISTERED LAND SURVEY, and duly authorized such action of the Commission by its Secretary. The prescribed ten-day period for appeal has elapsed without receipt of an appeal, as provided by Title 22, Section 598.320 of the Minneapolis Code or Ordinances.

Secretary of Planning Commission



MINNEAPOLIS, MINNESOTA
I hereby certify that this REGISTERED LAND SURVEY was approved by the City Planning Commission of the City of Minneapolis at a meeting thereof held this ____ day of _____, 20____.

Asst. City Clerk

RESIDENT AND REAL ESTATE SERVICES, Hennepin County, Minnesota
I hereby certify that taxes payable in 20____ and prior years have been paid for land described on this REGISTERED LAND SURVEY. Dated this ____ day of _____, 20____.

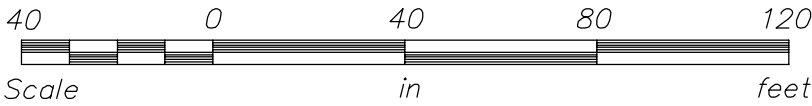
_____, Hennepin County Auditor By: _____
Deputy

SURVEY DIVISION, Hennepin County, Minnesota
Pursuant to MINN. STAT. Sec. 389.09, Subd. 1, this REGISTERED LAND SURVEY has been approved this ____ day of _____, 20____.

_____, Hennepin County Surveyor By: _____
County Surveyor

REGISTRAR OF TITLES, Hennepin County, Minnesota
I hereby certify this REGISTERED LAND SURVEY NO. _____ was filed for record in this office this ____ day of _____, 20____, at ____ o'clock ____ M.

_____, Registrar of Titles By: _____
Deputy



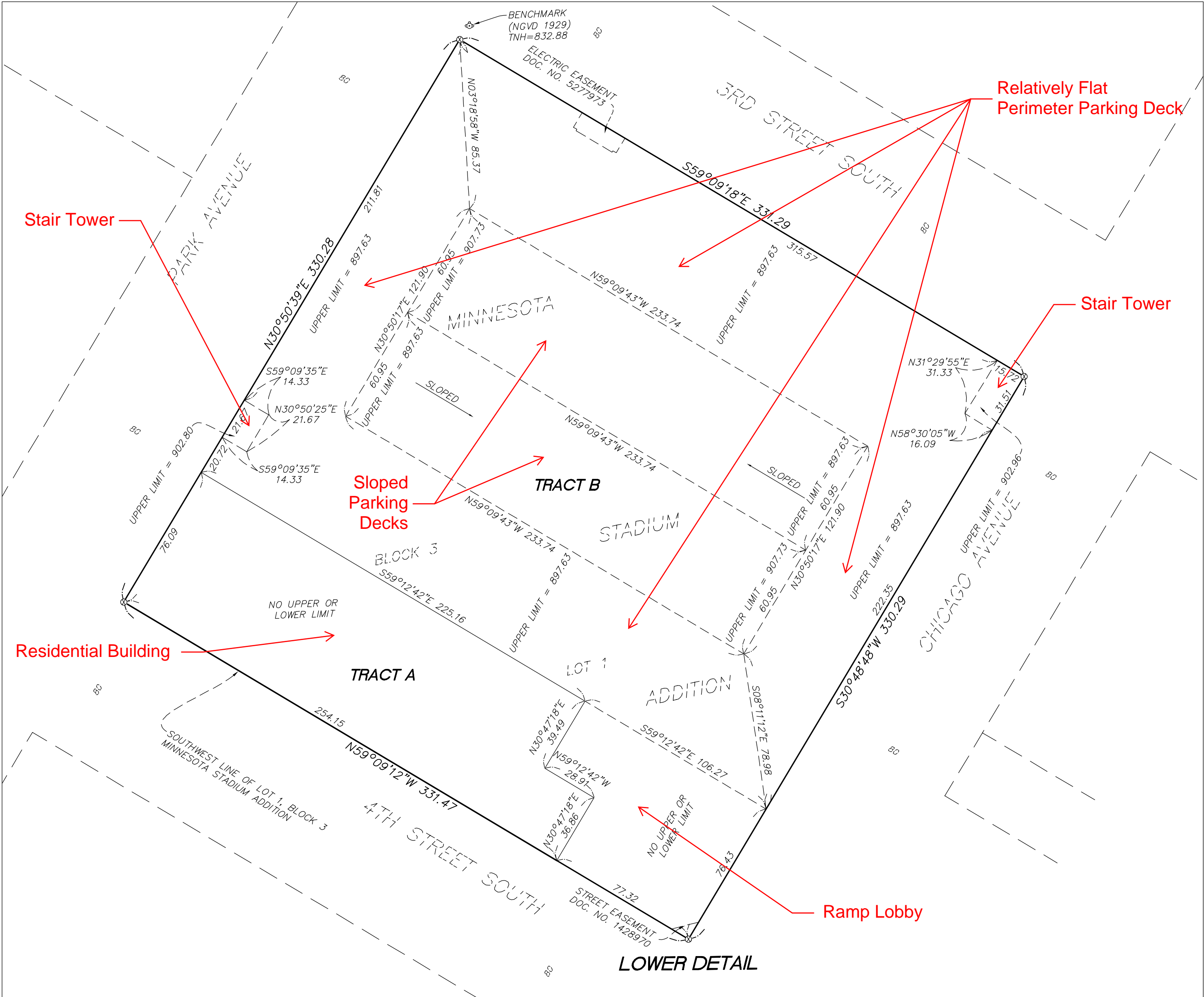
The Southwest line of Lot 1, Block 3,
MINNESOTA STADIUM ADDITION is assumed to
bear N59°09'12"W.

⊗ Denotes set chiseled "X"

Westwood
Professional Services, Inc.

REGISTERED LAND SURVEY NO.

R.T. DOC. NO.



I, Nathan H. Carlson, hereby certify that, in accordance with the provisions of Minnesota Statutes, Section 508.47, I have surveyed the following described property situated in the County of Hennepin, State of Minnesota.

Lot 1, Block 3, MINNESOTA STADIUM ADDITION.

Torrens Property
Certificate of Title No. 1425601

I hereby certify that this REGISTERED LAND SURVEY was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota and that this REGISTERED LAND SURVEY is a correct representation of said parcel of land. Dated this _____ day of _____, 20____.

Nathan H. Carlson, Land Surveyor
Minnesota License No. 45873

STATE OF MINNESOTA
COUNTY OF _____

The foregoing Surveyor's Certificate was acknowledged before me this _____ day of _____, 20__, by Nathan H. Carlson, Land Surveyor, Minnesota License No. 45873.

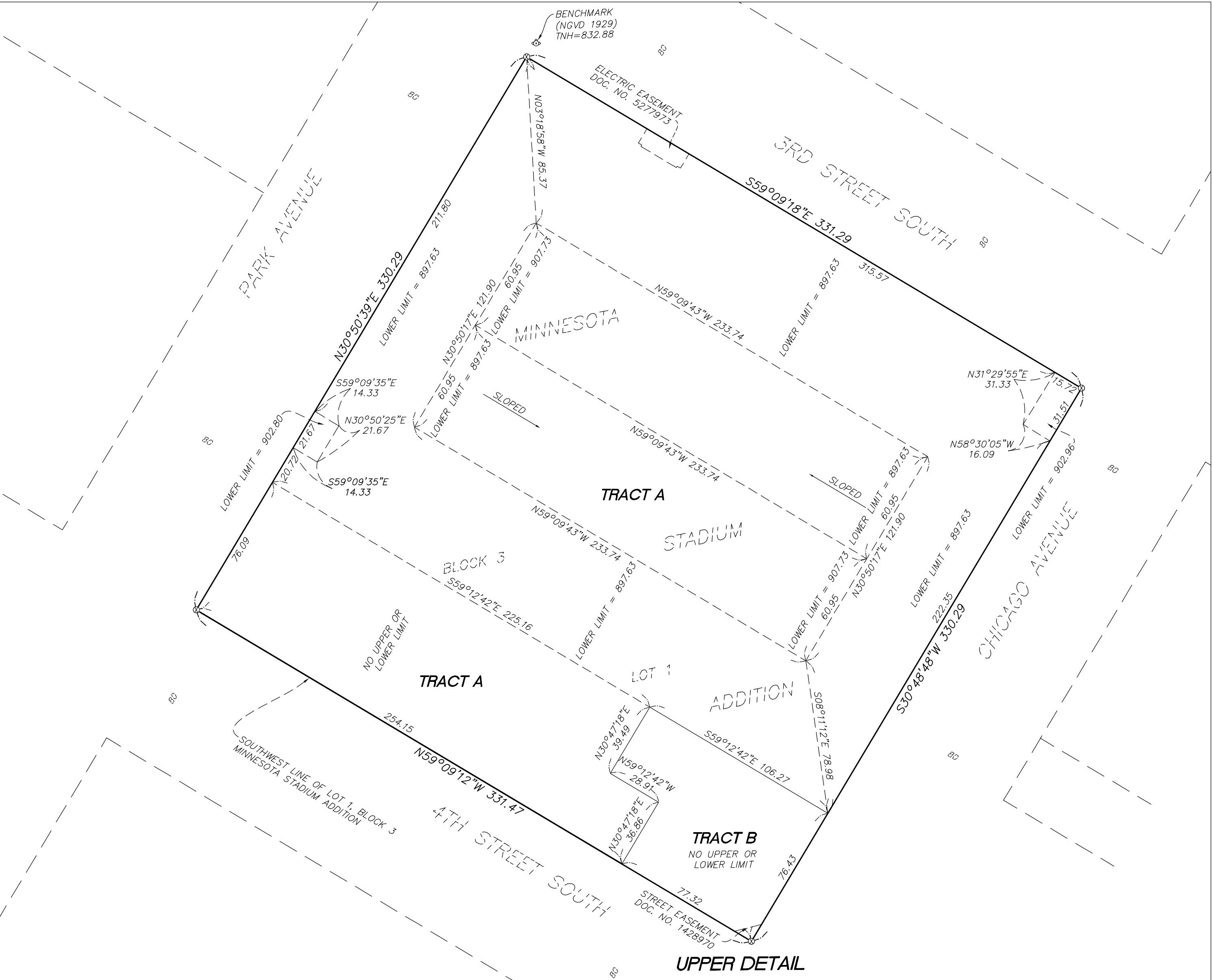
(Signature) _____ (Name Printed) _____

Notary Public, _____ County, Minnesota
My Commission Expires _____

PLANNING COMMISSION, MINNEAPOLIS, MINNESOTA

I, The Secretary of the Planning Commission of the City of Minneapolis, Minnesota, do hereby certify that on this _____ day of _____, 20____, the City of Minneapolis acting by and through its City Planning Commission duly approved this REGISTERED LAND SURVEY, and duly authorized such action of the Commission by its Secretary. The prescribed ten-day period for appeal has elapsed without receipt of an appeal, as provided by Title 22, Section 598.320 of the Minneapolis Code or Ordinances.

Secretary of Planning Commission



MINNEAPOLIS, MINNESOTA

I hereby certify that this REGISTERED LAND SURVEY was approved by the City Planning Commission of the City of Minneapolis at a meeting thereof held this _____ day of _____, 20____.

Asst. City Clerk

RESIDENT AND REAL ESTATE SERVICES, Hennepin County, Minnesota

I hereby certify that taxes payable in 20____ and prior years have been paid for land described on this REGISTERED LAND SURVEY. Dated this _____ day of _____, 20____.

_____, Hennepin County Auditor By: _____
Deputy

SURVEY DIVISION, Hennepin County, Minnesota

Pursuant to MINN. STAT. Sec. 389.09, Subd. 1, this REGISTERED LAND SURVEY has been approved this _____ day of _____, 20____.

_____, Hennepin County Surveyor By: _____
County Surveyor

REGISTRAR OF TITLES, Hennepin County, Minnesota

I hereby certify this REGISTERED LAND SURVEY NO. _____ was filed for record in this office this _____ day of _____, 20____, at _____ o'clock _____ M.

_____, Registrar of Titles By: _____
Deputy

Scale 40 0 40 80 120
in feet

The Southwest line of Lot 1, Block 3, MINNESOTA STADIUM ADDITION is assumed to bear N59°09'12"W.

⊗ Denotes set chiseled "X"

Westwood Professional Services, Inc.

Tract A: Residential Property
Tract B: MSFA Ramp Property



District Overview

In 2012, Ryan Companies, along with a host of community stakeholders, rolled out the vision for Downtown East on land formerly owned by the Star Tribune and occupied by surface parking lots. The vision was for a mixed use development incorporating office, retail, hospitality, and residential uses surrounding and activating a public green space now known as the Commons. Over the past 5 years that vision has largely been realized. The investment in Downtown East has been a catalyst for numerous additional developments on the east side of downtown, comprised of thousands of residential units and hotel rooms, as well as thousands of square feet of retail and restaurant space to support this urban neighborhood.

One component of the core Downtown East project was the construction of a 1,600 stall parking facility to serve US Bank Stadium. To ensure a use that would be a positive impact on the park itself, the majority of the south façade of the parking garage was kept back from the street, allowing for a development parcel to be contemplated later. Ryan was awarded the development rights for this parcel in 2014 and is excited to submit this proposal for a 26 story residential tower that will complete the vision of Downtown East and enhance the growing neighborhood that the previous phases of the development have helped anchor.

Project Overview

The liner parcel of the MSFA parking facility (Mills Fleet Farm Parking Ramp) presents a unique opportunity to complete the final piece of the Downtown East redevelopment and create a new active use between US Bank Stadium and the Wells Fargo towers. The proposed 26 story residential tower will add activity to the streetscape and Commons, bringing life from a full amenity floor on level 25, large windows and balconies of the residences on floors 2-24, all the way down to lobby and other amenities on South 4th Street. Furthermore, it will complete the skyline between the stadium and the fully developed parcels to the west.

Parking and Circulation

Since the liner parcel has a very limited footprint and is next to an existing parking ramp, the proposed strategy for parking and circulation is unique. The proposal seeks two curb cuts, one on Park Ave and another on 4th St, along with access from the parking garage on a number of levels above grade. The Park Ave curb cut has already been contemplated and accommodated for in the recently constructed Park Ave improvements. This will provide access to limited parking within the building at all times. An additional curb cut is proposed along 4th St. S in order to provide for trash and loading within the footprint of the building.

Parking garage connections are proposed on levels 4-6 of the existing ramp in order to take advantage of vehicle vertical circulation within the ramp, which could not be provided within the liner parcel itself. This will allow for access through a secure garage door to parking stalls within the footprint of the liner.

Architectural Character and Materials

Diagrammatically, the building is thought of as 2 volumes interacting. A glass volume is expressed on the top of the building, extending down to grade on the west portion of the site. A more traditional architectural precast volume is expressed on the north, east, and west facades and visually wraps the glass volume behind it. Unit balconies are organized and bundled to create a uniform language on the north and south facades. The vertical stacks of units are bundled in varying heights with a contrasting horizontal element to enhance the vertical expression of the building and visually break up the façade. Materials are intentionally limited for simplicity, consisting of: glass, light-colored integral color architectural precast, dark-colored integral color architectural precast, and dark metal panel.

Amenities

Amenities for the building are located in 4 areas. The lobby at the corner of 4th Street and Park Avenue extends along 4th with a linear active use element. The east end of the first floor includes a bike maintenance and electric scooter charging room, as well as a pet spa and outdoor pet exercise area. On the 6th floor, the amenity deck over the top of the south bay of the parking structure provides space for a gathering areas and walk-out unit patios. The top occupied floor of the building, Floor 25, is where the main amenities are located, including a pool deck on the west end where the massing steps back.

Overview

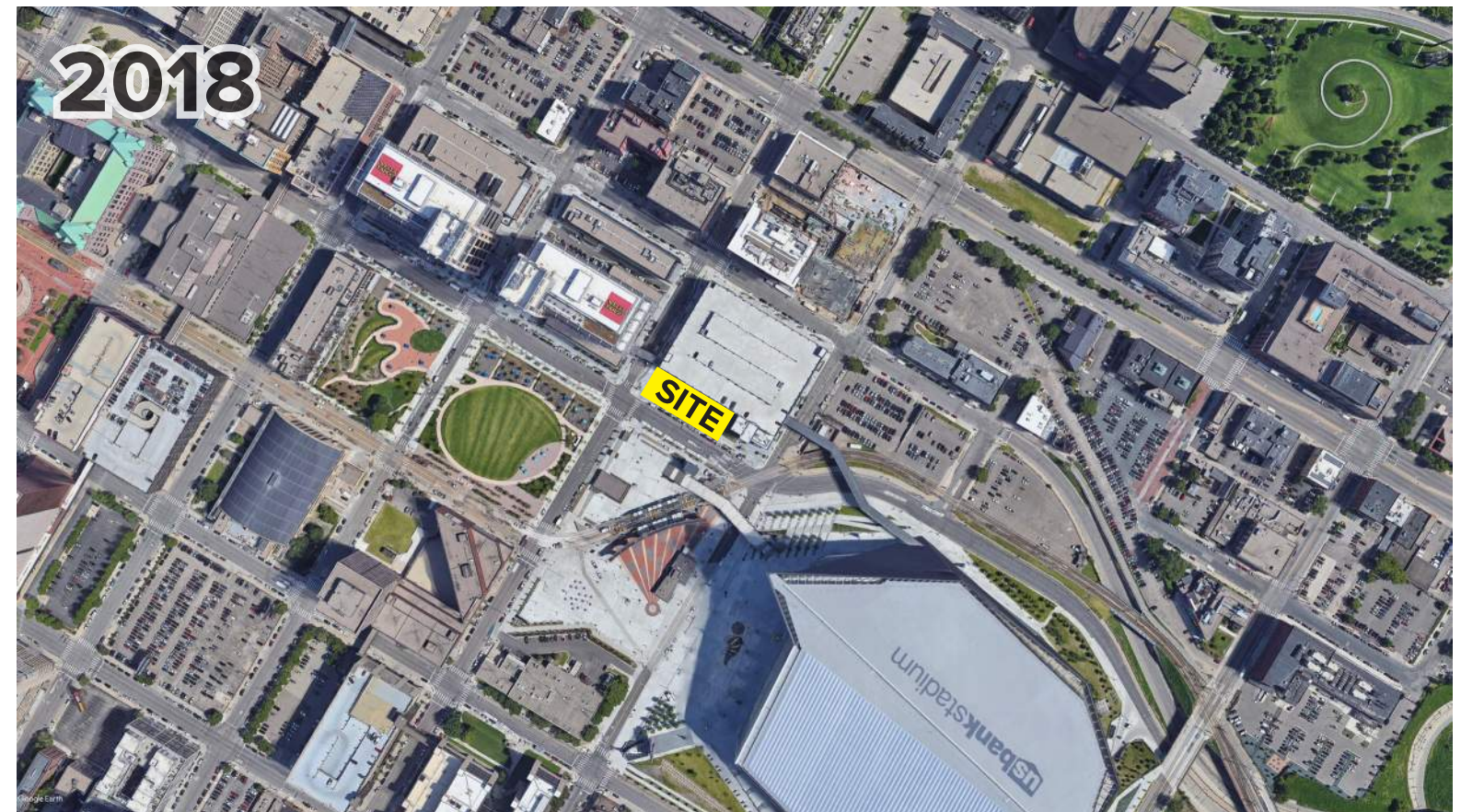
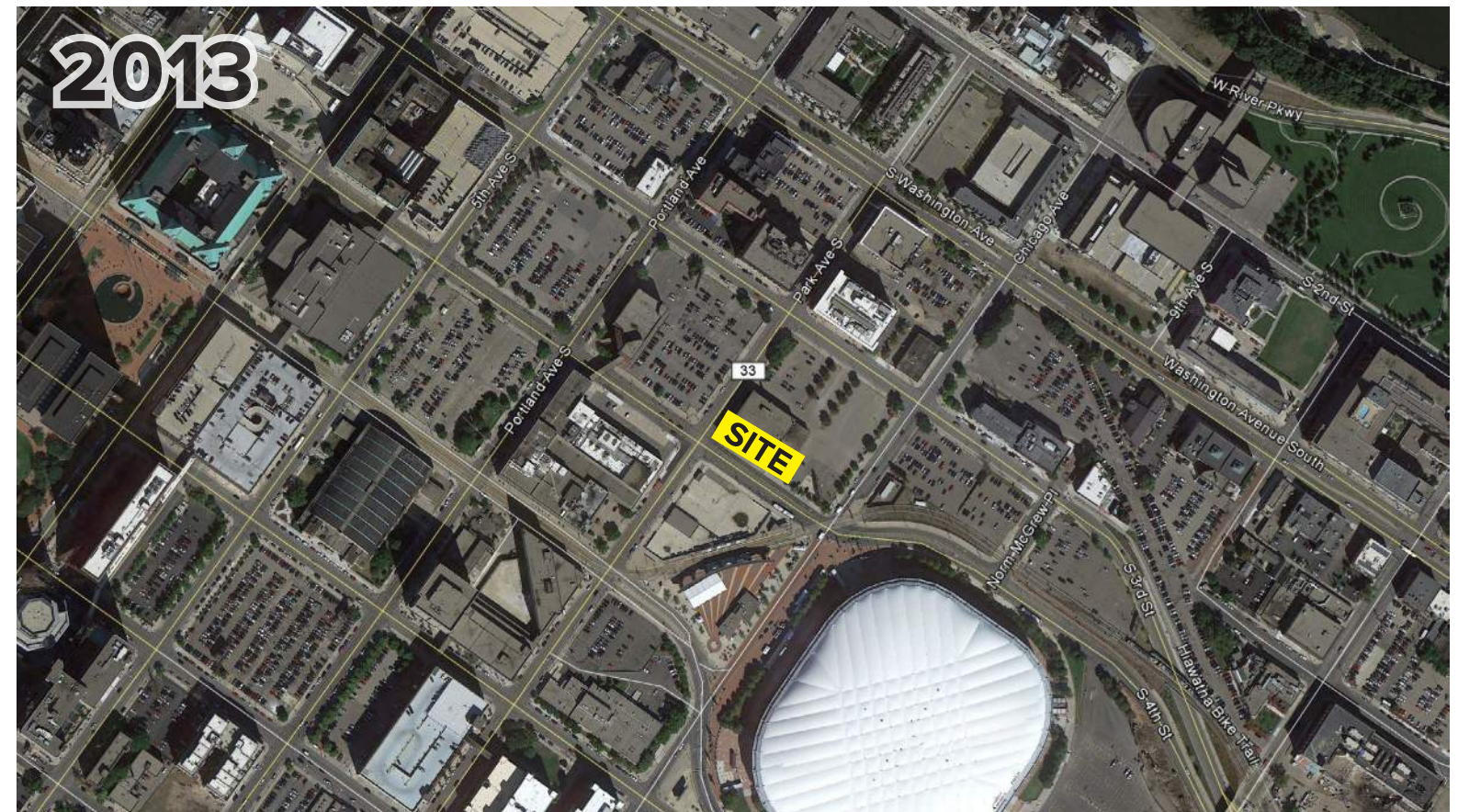
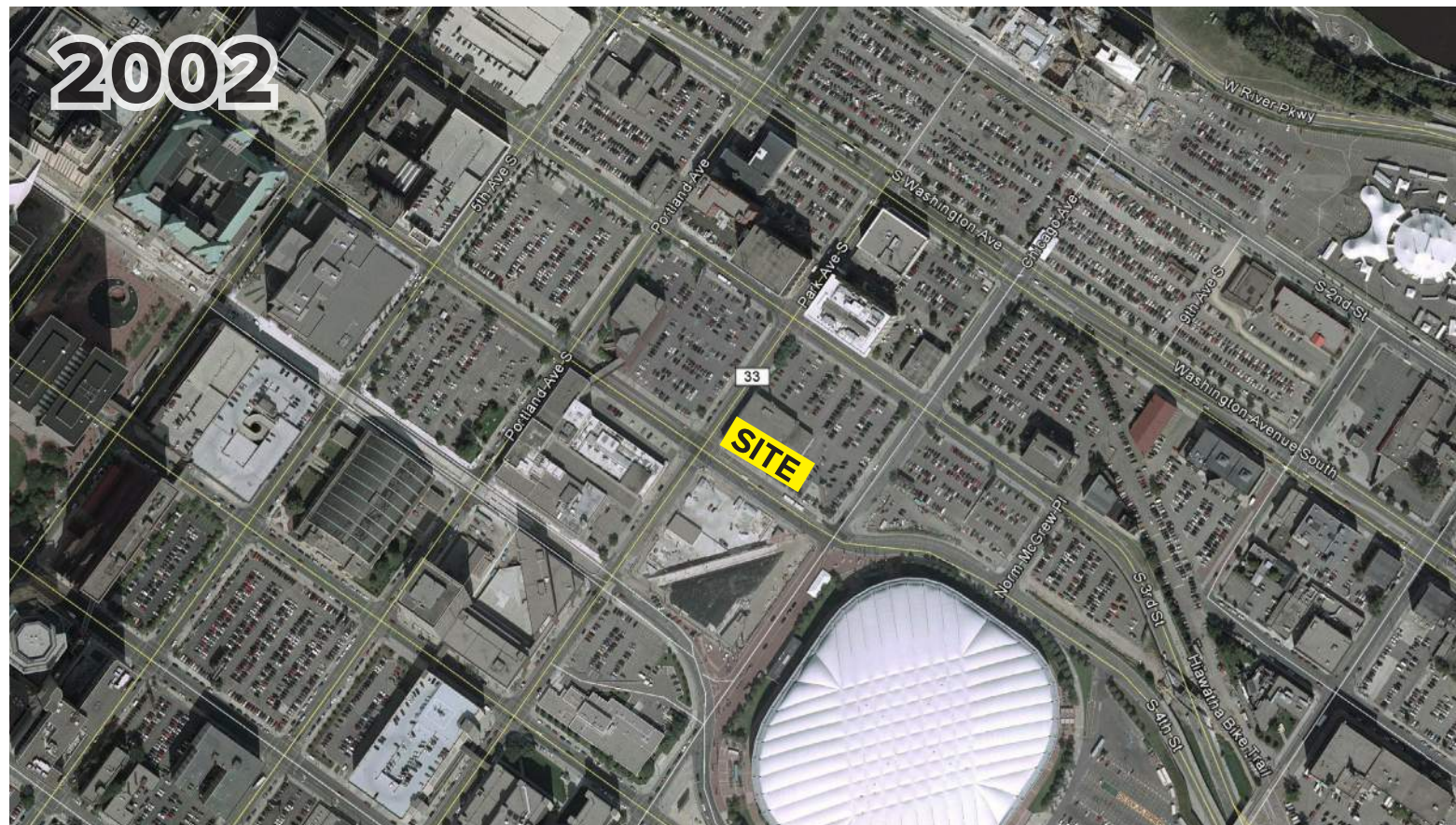
Project Statistics

Height: 305’
GFA: 319,475 GSF
*Does not include parking areas or balconies

Dwelling Units: 342
Parking Stalls: 120

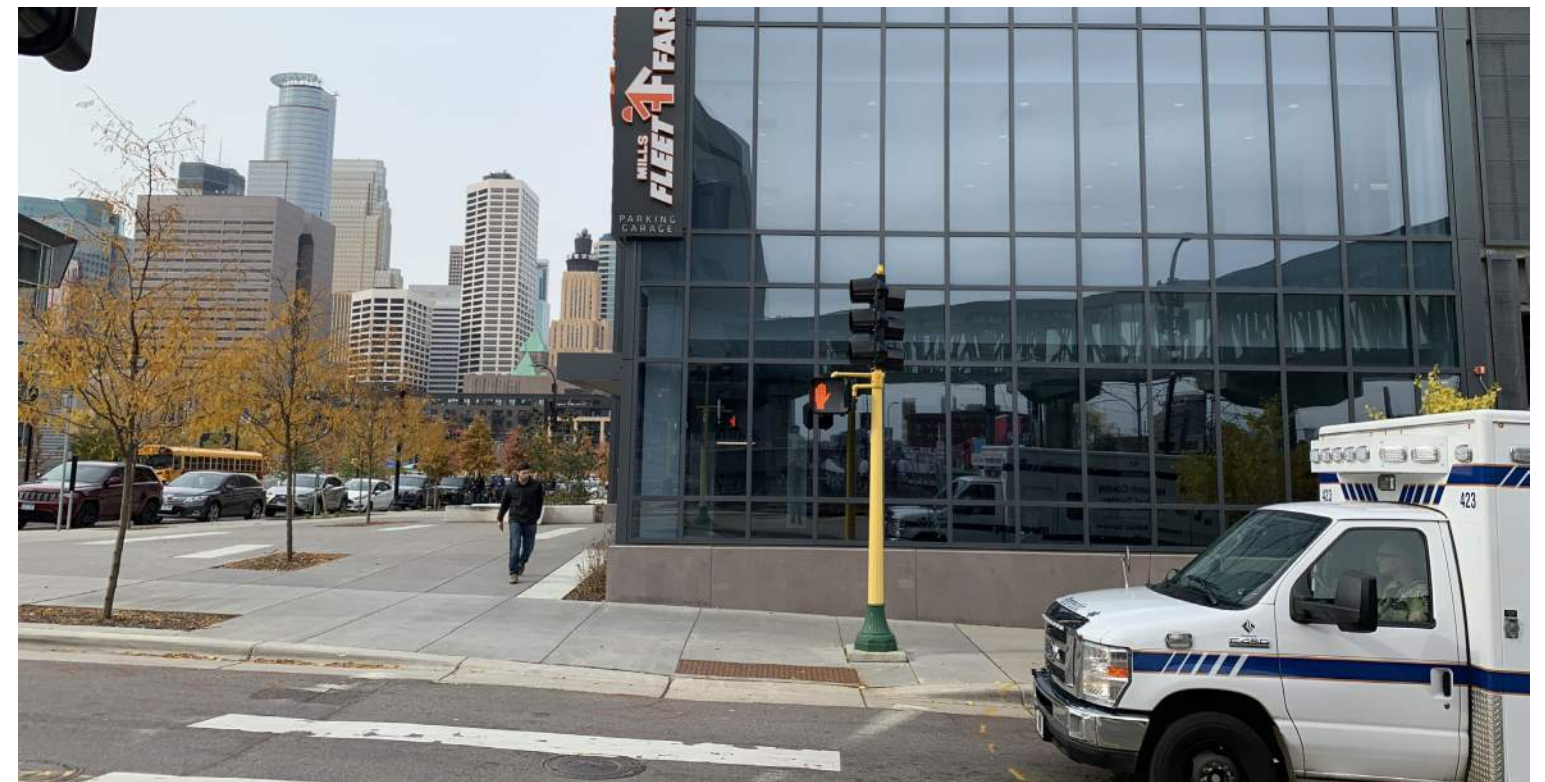
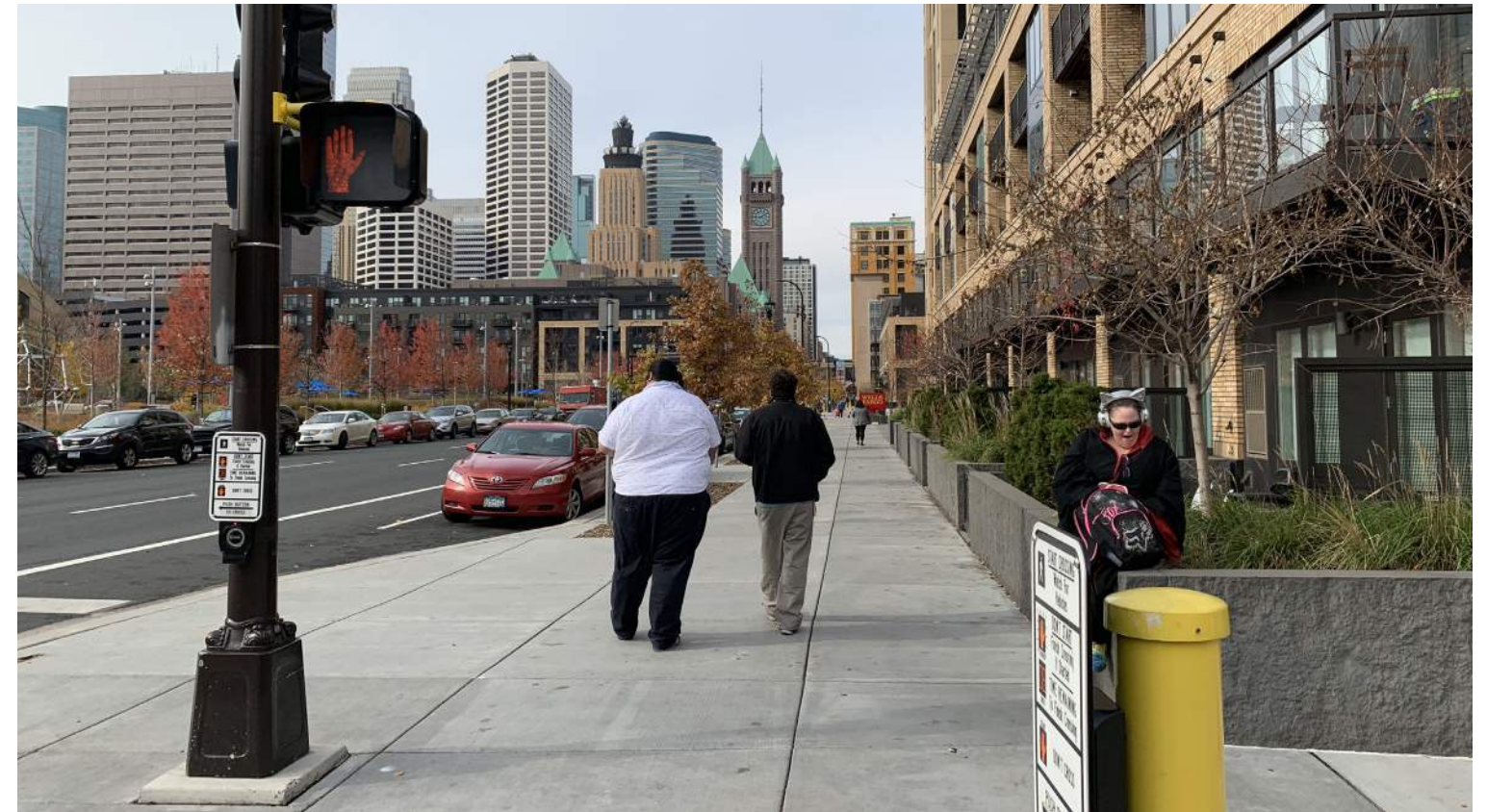
Proposed PUD Amenity Points
(5 required)

Shared Vehicles (3 points)
Recycling storage (1 point)
Pet Exercise Area (1 Point)



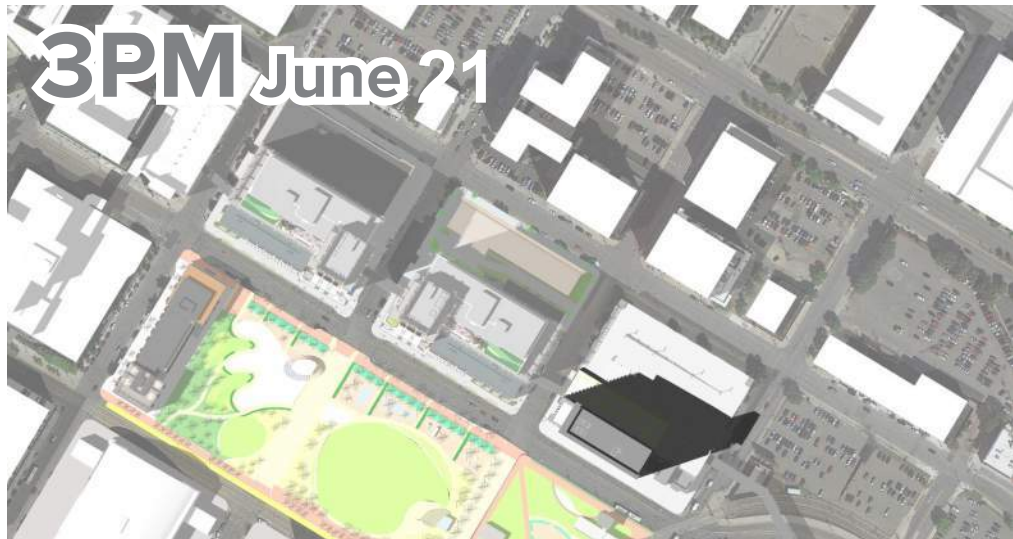


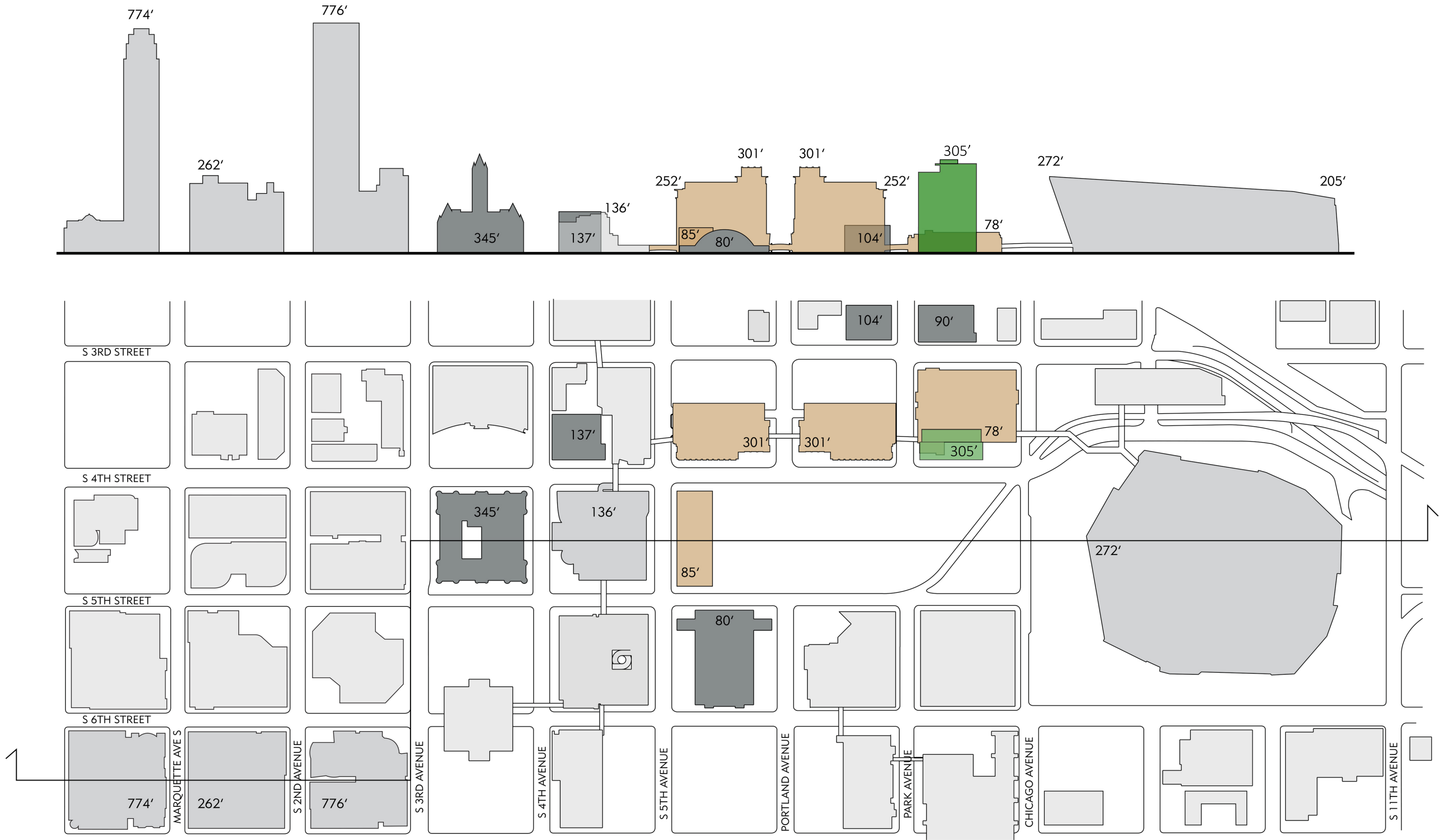
















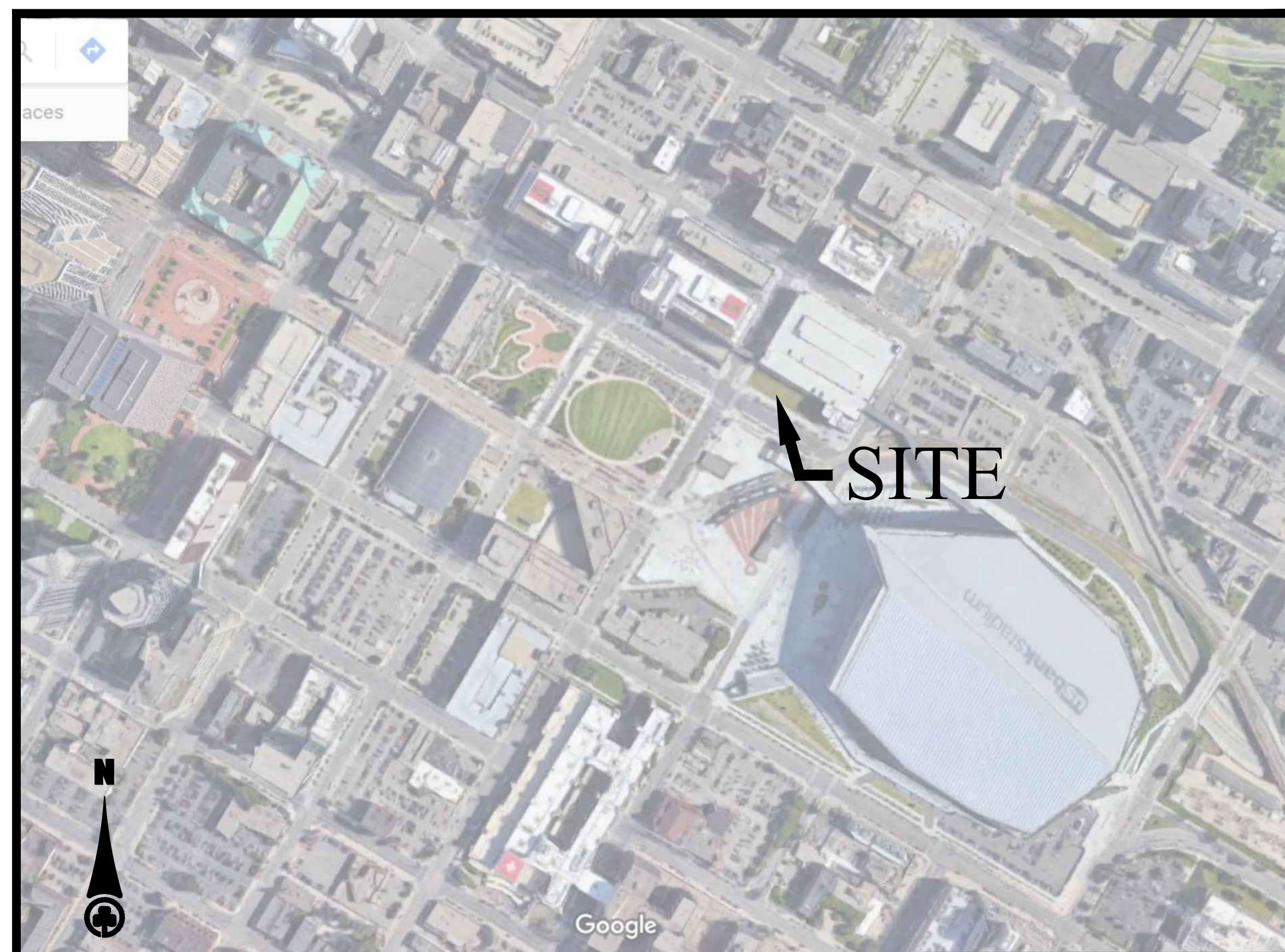


BLOCK 1 RESIDENTIAL

710 SOUTH 4TH STREET, MINNEAPOLIS, MN 55415

LUA SUBMITTAL

LOCATION MAP



SHEET INDEX

●	12/19/2018	C001	COVER SHEET
●	11/17/2018	C100	ALTA-NSPS LAND TITLE SURVEY
●	11/21/2018	C101	ALTA-NSPS LAND TITLE SURVEY
●	12/19/2018	C101	REMOVALS PLAN
●	12/19/2018	C200	EROSION CONTROL PLAN
●	12/19/2018	C300	SITE PLAN
●	12/19/2018	C400	GRADING PLAN
●	12/19/2018	C500	UTILITY PLAN
●	12/19/2018	C600	CIVIL DETAILS
●	12/19/2018	L100	LANDSCAPE SITE PLAN
●	12/19/2018	L500	LANDSCAPE DETAIL SHEET

DRAWING INDEX LEGEND

FILLED CIRCLE INDICATES DRAWING INCLUDED WITHIN THIS ISSUE MOST RECENT REVISION NUMBER MOST RECENT ISSUE OR REVISION DATE				
	-	X/XX/201X	CX	SHEET TITLE

PRELIMINARY
NOT FOR
CONSTRUCTION

PROJECT INFORMATION

**BLOCK 1
RESIDENTIAL**

710 SOUTH 4TH STREET
MINNEAPOLIS, MN 55415

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name _____

CHAD H. LOCKWOOD

REGISTRATION NO.	DATE
44986	

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DRAWN BY TJR
CHECKED BY CHL

JOB NO.
700-515

DATE
12/19/2018

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LUA SUBMITTAL
COVER SHEET

C001

CONSULTANTS

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10 SOUTH 4TH STREET
MINNEAPOLIS, MN 55415

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name _____

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JOB NO. 00-515	DATE 12/19/2018

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ALTA-NSPS
LAND TITLE
SURVEY

C101

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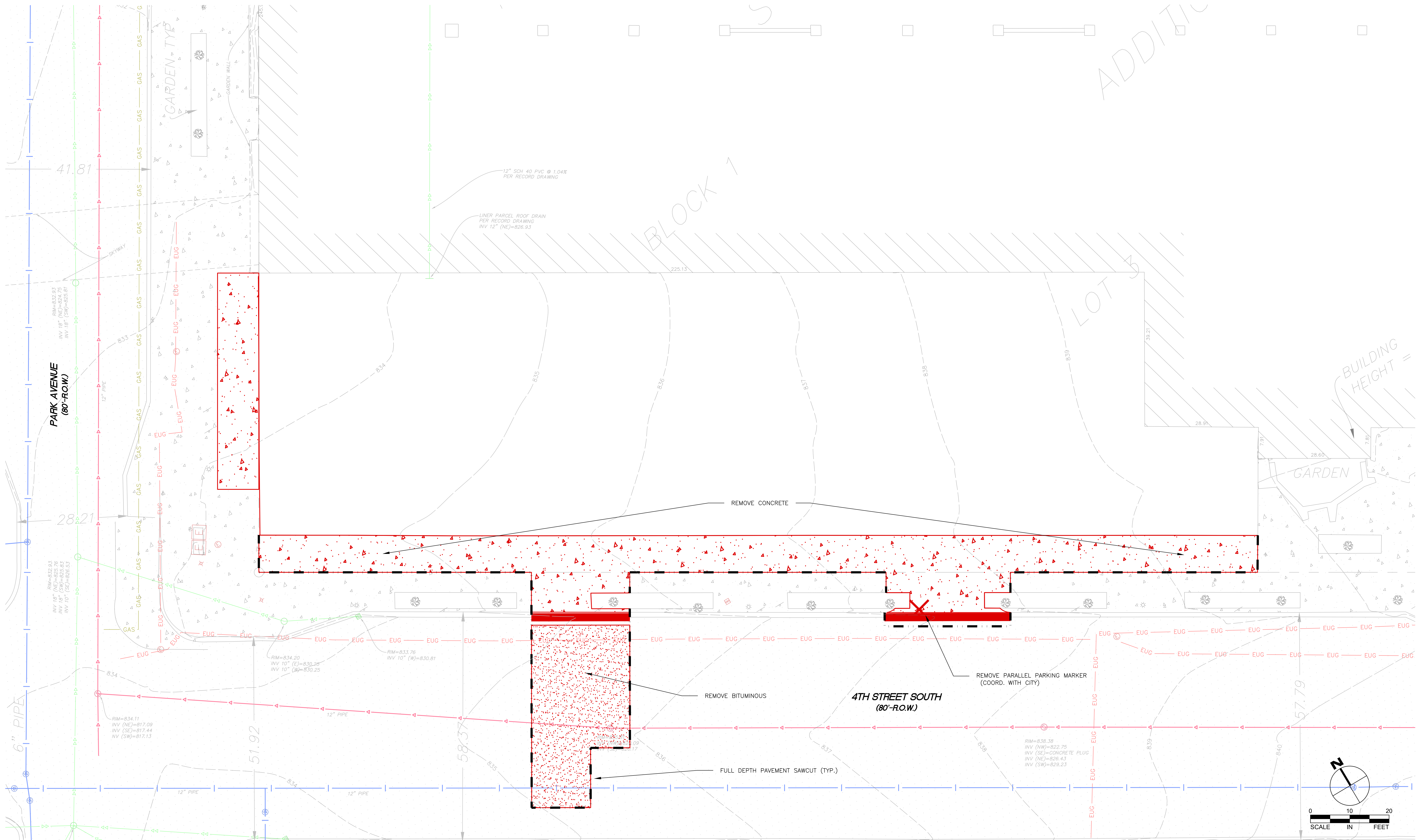
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JOB NO. 00-515	DATE 12/19/2018

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION
1	11/21/2018	PDR SUBMITTAL
2	12/19/2018	LUA SUBMITTAL
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LUA SUBMITTAL

REMOVALS PLAN

C101



REMOVAL NOTES

1. CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO THE COMMENCEMENT OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
2. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 1-800-252-1166 OR 811) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO EXCAVATION.
3. ALL EROSION CONTROL MEASURES TO BE INSTALLED PRIOR TO REMOVALS.
4. CONTRACTOR TO REMOVE/RELOCATE EXISTING PRIVATE UTILITIES AS NECESSARY. CONTRACTOR TO COORDINATE ACTIVITIES WITH UTILITY COMPANIES.
5. CONTRACTOR IS RESPONSIBLE FOR ALL REMOVALS NOTED ON PLANS. ALL PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
6. CONTRACTOR IS RESPONSIBLE FOR PROTECTING, OR REMOVING AND REINSTALLING ALL FENCES, SIGNS, AND OTHER MISCELLANEOUS ITEMS. SERVICE MUST BE MAINTAINED THROUGHOUT CONSTRUCTION. TEMPORARY OR PERMANENT RELOCATION OF ABOVE ITEMS MAY BE NECESSARY AND SHALL BE COORDINATED WITH THE APPROPRIATE PARTY. ALL WORK REQUIRED TO COMPLETE THE ABOVE SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
7. CARE MUST BE TAKEN DURING CONSTRUCTION AND EXCAVATION TO PROTECT ANY SURVEY MONUMENTS AND/OR PROPERTY IRONS.

LEGEND

REMOVAL	
	CURB
	BITUMINOUS
	HEAVY DUTY BITUMINOUS
	CONCRETE
	GRAVEL
	BUILDING
	CHAIN LINK FENCE
	WOOD FENCE
	BLOCK RETAINING WALL
	STONE RETAINING WALL
	SAWCUT
	ABANDON IN PLACE
	UTILITY
	BULKHEAD
	EACH

PRELIMINARY
NOT FOR
CONSTRUCTION

PROJECT INFORMATION

BLOCK 1 RESIDENTIAL

710 SOUTH 4TH STREET
MINNEAPOLIS, MN 55415

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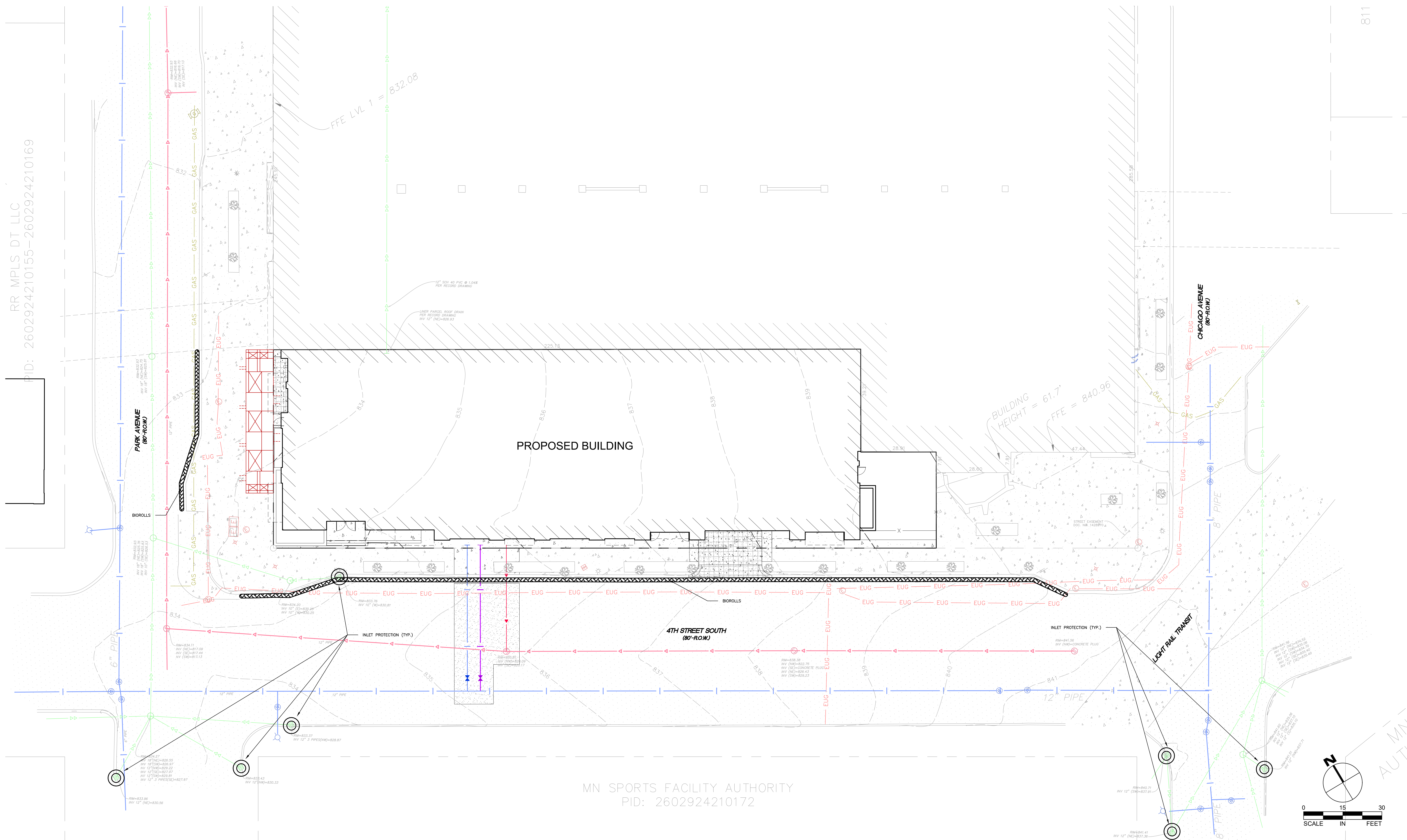
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TJR	CHL
JOB NO.	DATE
700-515	12/19/2018

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LUA SUBMITTAL

EROSION CONTROL PLAN

C200

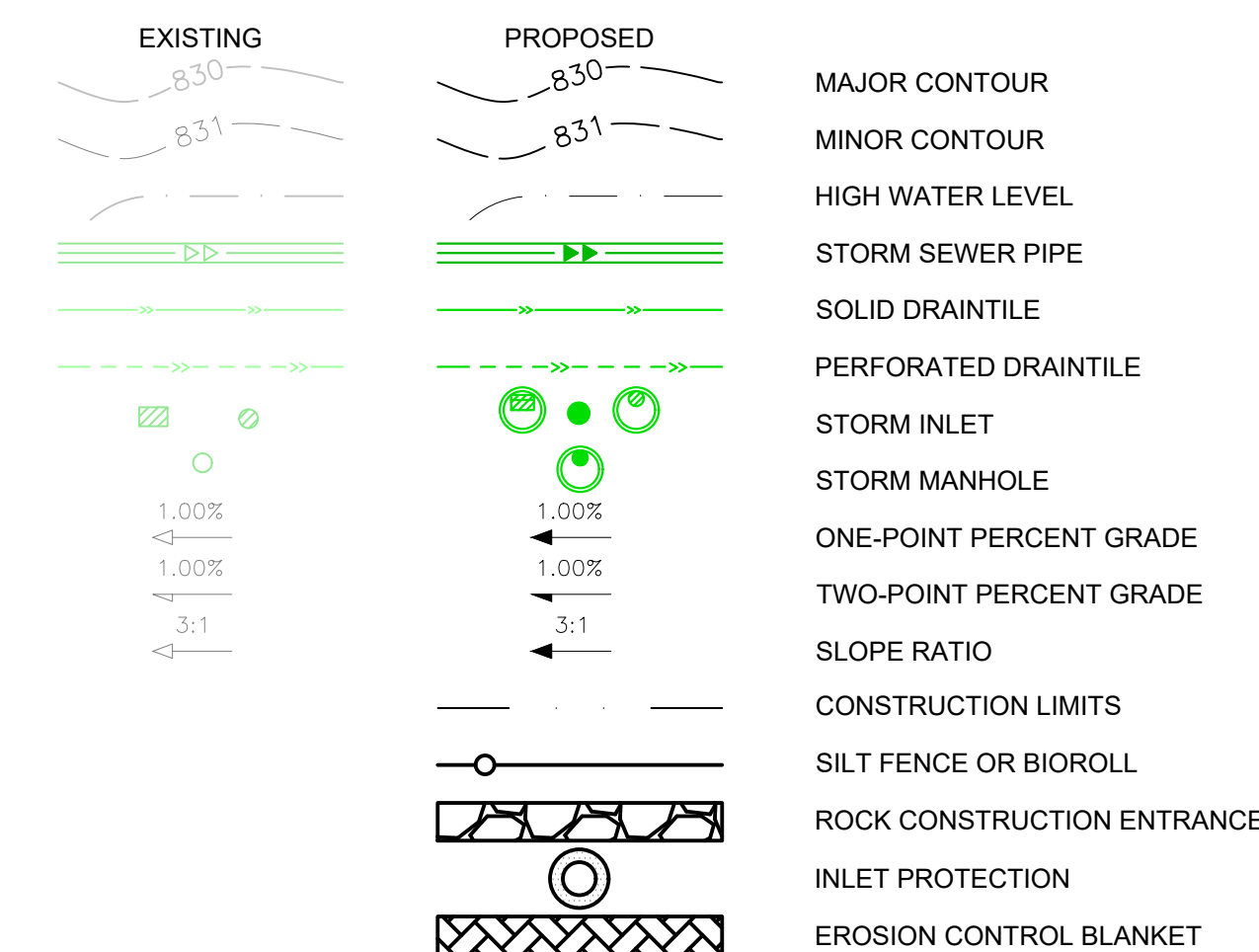


EROSION CONTROL NOTES:

1. CONTRACTOR MUST CALL A CONSTRUCTION START 48 HOURS PRIOR TO ANY LAND DISTURBANCES (612-673-3867). FAILURE TO DO SO MAY RESULT IN FINES. THE REVOCATION OF PERMIT AND STOP WORK ORDER BEING ISSUED.
2. INSTALL PERIMETER EROSION CONTROL AND ROCK CONSTRUCTION ENTRANCE AT LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES.
3. CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES IN ACCORDANCE WITH CITY AND STATE PERMITS AS REQUIRED.
4. THE CONTRACTOR, DEVELOPER AND OWNER SHALL REQUEST THE EROSION CONTROL INSPECTOR TO INSPECT AND APPROVE WORK COMPLETED IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENT CONTROL PLAN, AND IN ACCORDANCE WITH THE ORDINANCE. THE CONTRACTOR, DEVELOPER OR OWNER SHALL BE REQUIRED TO OBTAIN WRITTEN APPROVAL BY THE INSPECTOR AT THE COMPLETION OF DEVELOPMENT AS OUTLINED IN SECTION 52.260 SUBSECTIONS (1), (2) AND (3).

	ESTIMATED QUANTITIES
SILT FENCE OR BIOROLL (LF)	400
ROCK CONSTRUCTION ENTRANCE (SF)	2,000
INLET PROTECTION (EACH)	7

LEGEND:



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MINNEAPOLIS, MN 55415

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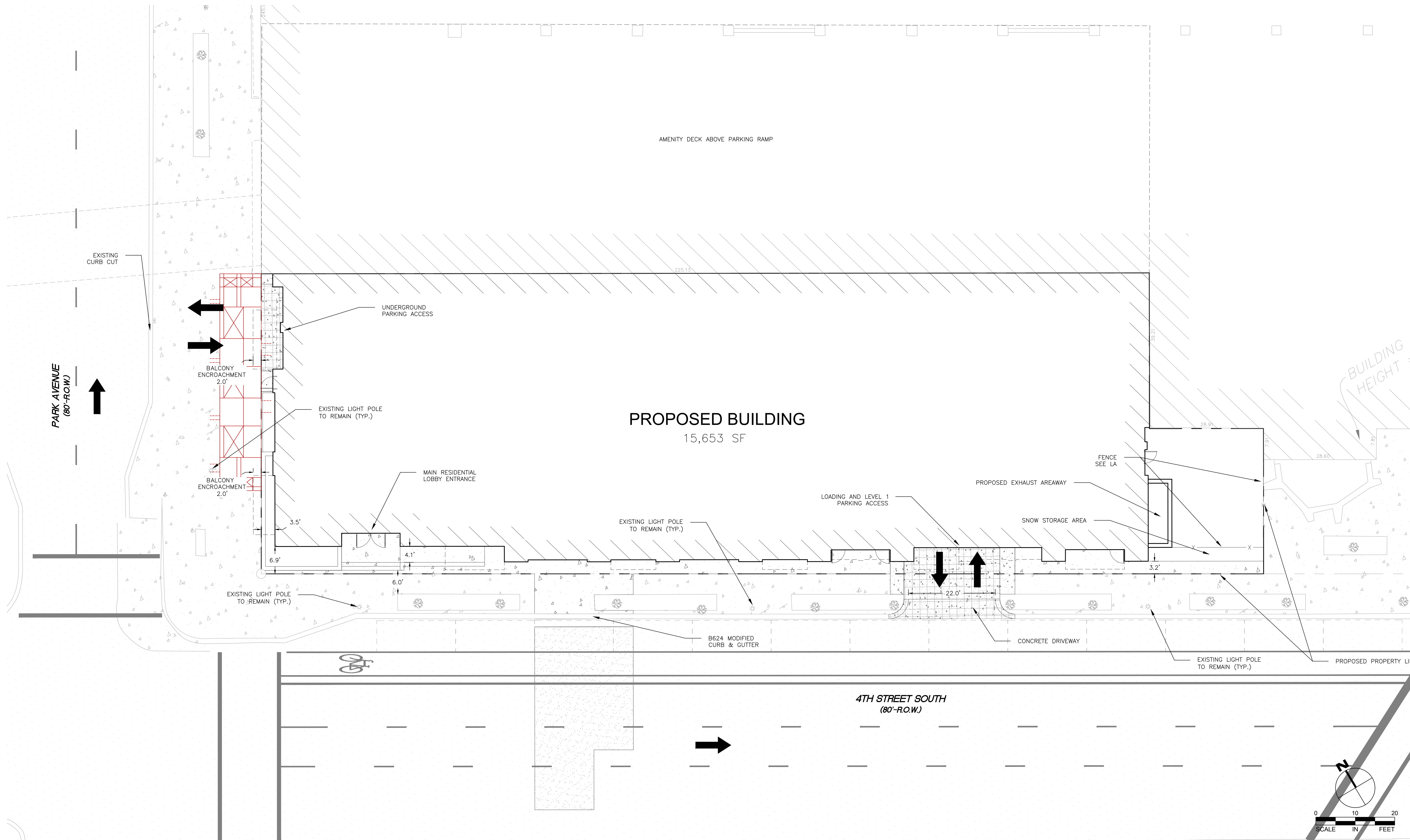
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JOB NO.	DATE
700-515	12/19/2018

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SITE PLAN

C300



CITY OF MINNEAPOLIS NOTES

1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF MINNEAPOLIS SPECIFICATIONS AND BUILDING PERMIT REQUIREMENTS.
2. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT. PLEASE CONTACT DAVE PREHALL AT (612) 673-5759 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATION WILL BE REQUIRED TO BE REINSTALLED AT OWNER EXPENSE.
3. AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER AT (612) 673-2743 REGARDING DETAILS OF SIDEWALK AND LAND CLOSURES. LOOK UP ON WWW.MINNEAPOLIS.MN.ROWAY.NET/ FOR A PERMIT.
4. ANY METERED PARKING SPACES NEEDED TO FACILITATE PROJECT WORK MUST BE HOODED AT PROJECT EXPENSE FOR THE LENGTH OF DISRUPTION REQUIRED. IF SIDEWALK IS REMOVED AND PARKING LANE IS CLOSED, THE METER AND SPACE DELINEATORS MUST BE REMOVED AND REINSTALLED UPON PROJECT COMPLETION AT PROJECT EXPENSE. REQUEST FOR HOODS MUST BE MADE AT THE SAME TIME THAT THE "OBSTRUCTION PERMIT" IS OBTAINED VIA WWW.MINNEAPOLIS.MN.ROWAY.NET. CONTACT SCOTT KRAMER AT SCOTT.KRAMER@MINNEAPOLIS.GOV IF YOU HAVE ANY QUESTIONS REGARDING OBSTRUCTION PERMITS OR METER REMOVAL.
5. CONTACT ALLAN KLUGMAN AT (612) 673-2743 FIRST FOR CONSTRUCTION FOR THE TEMPORARY REMOVAL/TEMPORARY RELOCATION OF ANY CITY OF MINNEAPOLIS LIGHTING OR TRAFFIC SIGNAL SYSTEM THAT MAY BE IN THE WAY OF CONSTRUCTION.
6. PAVEMENT MARKINGS THAT EXIST PRIOR TO THE START OF THE PROJECT MUST BE PLACED IN KIND, IF IT IS LATEX OR EPOXY, REPLACE THEN IT MUST GO BACK IN WITH THE SAME TYPE OF PAINT. ALL WORK TO BE DONE BY THE CITY OF MINNEAPOLIS TRAFFIC DIVISION. ALL COSTS ASSOCIATED WITH PAVEMENT MARKING REPLACEMENT SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
7. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES INCLUDING TRAFFIC SIGNAL SYSTEMS, STREET LIGHTING, TRAFFIC SIGNS, PARKING METERS, AND PAVEMENT MARKINGS SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
8. CONTACT DOUG MADAY AT (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS TRAFFIC SIGNS OR PAVEMENT MARKINGS THAT MAY BE IN THE WAY OF CONSTRUCTION.

SITE NOTES

1. CONTRACTOR MUST FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING CONDITIONS. THIS WOULD INCLUDE ALL EXISTING UTILITIES AND TOPOGRAPHIC FEATURES. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
2. ALL DIMENSIONS SHOWN ARE TO FACE OF CURB, FACE OF BUILDING, EDGE OF PAVEMENT, OR TO PROPERTY LINE, UNLESS OTHERWISE NOTED.
3. ALL PAVING, CONCRETE CURB, GUTTER AND SIDEWALK SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE DETAILS SHOWN PER THE DETAIL SHEET(S) AND STATE/LOCAL JURISDICTION REQUIREMENTS.
4. CONTRACTOR MUST FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING CONDITIONS. THIS WOULD INCLUDE ALL EXISTING UTILITIES AND TOPOGRAPHIC FEATURES. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
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6. ACCESSIBLE PARKING AND ACCESSIBLE ROUTES SHALL BE PROVIDED PER CURRENT ADA STANDARDS AND LOCAL/STATE REQUIREMENTS.
7. ALL RADI ARE TO GUTTER LINE OR PAVEMENT EDGE UNLESS OTHERWISE NOTED.
8. ALL CONCRETE CURB AND GUTTER RADI SHALL BE 5.0' UNLESS OTHERWISE NOTED.
9. ALL CURB TERMINI TO HAVE A 3 FOOT TAPER UNLESS OTHERWISE NOTED.
10. BITUMINOUS IMPREGNATED FIBER BOARD TO BE PLACED AT FULL DEPTH OF CONCRETE ADJACENT TO EXISTING STRUCTURES AND BEHIND CURB ADJACENT TO DRIVEWAYS AND SIDEWALKS.
11. SITE LIGHTING DESIGN PROVIDED BY OTHERS
12. PROPERTY TO USE SAME SNOW STORAGE AND REMOVAL AS THE EXISTING PARKING RAMP. SNOW STORAGE ROOM LOCATED ON NORTHWEST CORNER OF EXISTING RAMP.
















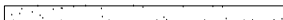


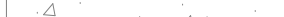



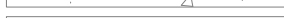
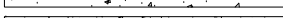

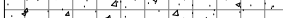
SIGNAGE AND STRIPING NOTES

1. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SITE SIGNAGE AND STRIPING AS SHOWN ON THIS PLAN.
2. CONTRACTOR SHALL PAINT ALL STALLS, ACCESSIBLE STALLS, LOGOS AND CROSS HATCH LOADING AISLES WITH WHITE PAVEMENT MARKING PAINT, 4" IN WIDTH.
3. ALL SIGNAGE SHALL INCLUDE POST, CONCRETE FOOTING AND STEEL CASING WHERE REQUIRED.
4. ALL SIGNAGE NOT PROTECTED BY CURB, LOCATED IN PARKING LOT OR OTHER PAVED AREAS TO BE PLACED IN STEEL CASING, FILLED WITH CONCRETE AND PAINTED YELLOW. REFER TO DETAIL.
5. ANYVAIL STOP SIGNS TO INCLUDE A 24" WIDE PAINTED STOP BAR IN WHITE PAINT, PLACED AT THE STOP SIGN LOCATION, A MINIMUM OF 4" FROM CROSSWALK IF APPLICABLE. ALL STOP BARS SHALL EXTEND FROM DIRECTIONAL TRANSITION BETWEEN LANES TO CURB.
6. ALL SIGNS TO BE PLACED 18" BEHIND BACK OF CURB UNLESS OTHERWISE NOTED.

SITE DATA

PRIMARY ZONING DISTRICT	BAN: DOWNTOWN NEIGHBORHOOD DISTRICT
OVERLAY ZONING DISTRICTS	DP DOWNTOWN PARKING DH DOWNTOWN HEIGHT
EXISTING PROPERTY AREA	2.51± AC
PROPOSED LOT 1 AREA	0.42 AC
PROPOSED LOT 2 AREA	2.09 AC
TOTAL	2.51 AC
EXISTING LOT 1 IMPERVIOUS AREA = 0.05 AC (11.9%)	
PROPOSED LOT 1 IMPERVIOUS AREA = 0.40 AC (95.2%)	
*NUMBERS DISREGARD ANY PERVIOUS AREA ON THE AMENITY DECK OVER LOT	
PROPOSED BUILDING FOOTPRINT	15,114± SF
PROPOSED BUILDING GFA	303,500± SF
TOTAL STRUCTURED PARKING STALLS = 105	
PARKING STALL DIMENSIONS = 8'5"X18'	
DRIVE AISLE WIDTHS = 22'	

LEGEND

EXISTING	PROPOSED	
		PROPERTY BOUNDARY
		LOT LINE
		EASEMENT
		CONSTRUCTION LIMITS
		CONCRETE CURB AND GUTTER
		BITUMINOUS PAVEMENT
		4" CONCRETE SIDEWALK
		6" CONCRETE SIDEWALK
		8" CONCRETE DRIVEWAY
		LIGHT POLE
		TRAFFIC SIGNAL
		SIGN
		BOLLARD

PRELIMINARY
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PROJECT INFORMATION

**BLOCK 1
RESIDENTIAL**

710 SOUTH 4TH STREET
MINNEAPOLIS, MN 55415

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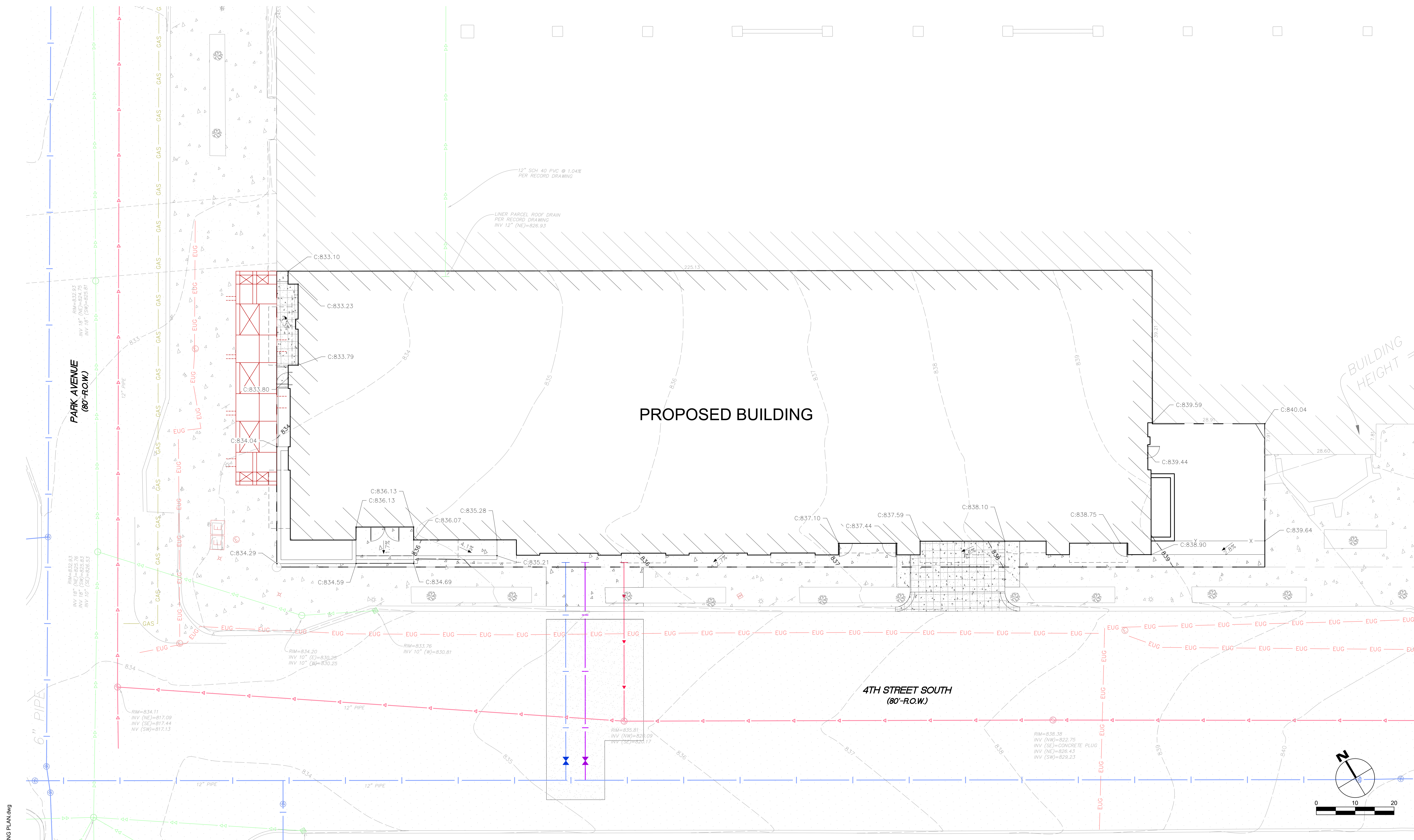
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JOB NO. 700-515	DATE 12/19/2018

[illegible]LUA
SUBMITTAL

GRADING PLAN

C400



GRADING NOTES:

1. SUBCONTRACTORS WHOSE WORK REQUIRES EXCAVATION SHALL CONTACT "GOPHER STATE ONE-CALL" (651-454-0002 OR 1-800-252-1166 OR 811) FOR UTILITY LOCATIONS 48 HOURS PRIOR TO EXCAVATION. HE/SHE SHALL REPAIR OR REPLACE DAMAGED UTILITIES DURING CONSTRUCTION AT NO COST TO OWNER OR GENERAL CONTRACTOR.
 2. EXISTING TOPOGRAPHY PER WESTWOOD PROFESSIONAL SERVICES SURVEY, DATED 11/20/18.
 3. REFER TO THE REPORT OF GEOTECHNICAL EXPLORATION AND REVIEW, DATED 09/26/14 AS PREPARED BY AMERICAN ENGINEERING TESTING, INC., FOR AN EXISTING SUBSURFACE SITE CONDITION ANALYSIS AND CONSTRUCTION RECOMMENDATIONS.
 4. GROUNDWATER WAS ENCOUNTERED IN THE SOIL BORINGS AT DEPTHS 39 TO 41.5 FEET BELOW GRADE. REFER TO GEOTECHNICAL REPORT FOR DETAILS.
 5. CONTRACTOR MUST FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING CONDITIONS. THIS WOULD INCLUDE ALL EXISTING UTILITIES AND TOPOGRAPHIC FEATURES. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
 6. SUITABLE GRADING MATERIAL SHALL CONSIST OF ALL SOIL ENCOUNTERED ON THE SITE WITH EXCEPTION OF TOPSOIL, DEBRIS, ORGANIC MATERIAL AND OTHER UNSTABLE MATERIAL AS DIRECTED BY THE GEOTECHNICAL ENGINEER. STOCKPILE TOPSOIL AND GRANULAR FILL AT LOCATIONS DIRECTED BY GENERAL CONTRACTOR.
 7. SUBGRADE EXCAVATION SHALL BE BACKFILLED IMMEDIATELY AFTER EXCAVATION TO HELP OFFSET ANY STABILITY PROBLEMS DUE TO WATER SEEPER OR STEEP SLOPES. WHEN PLACING NEW SURFACE MATERIAL ADJACENT TO EXISTING PAVEMENT, THE EXCAVATION SHALL BE BACKFILLED PROMPTLY TO AVOID UNDERMINING OF THE EXISTING PAVEMENT.
 8. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL HORIZONTAL AND VERTICAL CONTROL.
 9. PROPOSED GRADES SHOWN ARE FINISHED GRADES.
 10. ALL EXCESS MATERIAL, BITUMINOUS SURFACING, CONCRETE ITEMS, ANY ABANDONED UTILITY ITEMS, AND OTHER UNSTABLE MATERIALS SHALL BECOME THE PROPERTY OF THE SUBCONTRACTOR AND SHALL BE DISPOSED OFF THE CONSTRUCTION SITE.
 11. EARTHWORK & PAVING CONTRACTORS ARE RESPONSIBLE FOR GRADING AND SLOPING THE FINISHED GROUND SURFACE TO PROVIDE SMOOTH & UNIFORM SLOPES, WHICH PROVIDE POSITIVE DRAINAGE AWAY FROM THE BUILDING AND PREVENT FONDING IN LOWER AREAS. ANY DERIVATION FROM PLANS SHALL BE APPROVED BY THE PROJECT ENGINEER.
 12. GRADING BEHIND CURBS SHALL START AT THE TOP OF CURB.
 13. VERIFY THAT EXISTING UTILITIES HAVE ADEQUATE COVER TO MEET PROVIDER REQUIREMENTS IN AREAS OF GRADE REVISIONS.
 14. RETURN ALL TEMPORARY DIVERSIONS TO EXISTING GRADE UPON FINAL STABILIZATION.
 15. CONTRACTORS TO FOLLOW XCEL SAFETY GUIDELINES INSIDE EASEMENT AREAS.
 16. REMOVE SEDIMENT FROM EXISTING STORM SEWER IF PRESENT.
 17. SPOT ELEVATIONS REPRESENT FINISHED SURFACE GRADES, GUTTER/FLOW LINE/EDGE OF BUILDING, OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 18. CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED

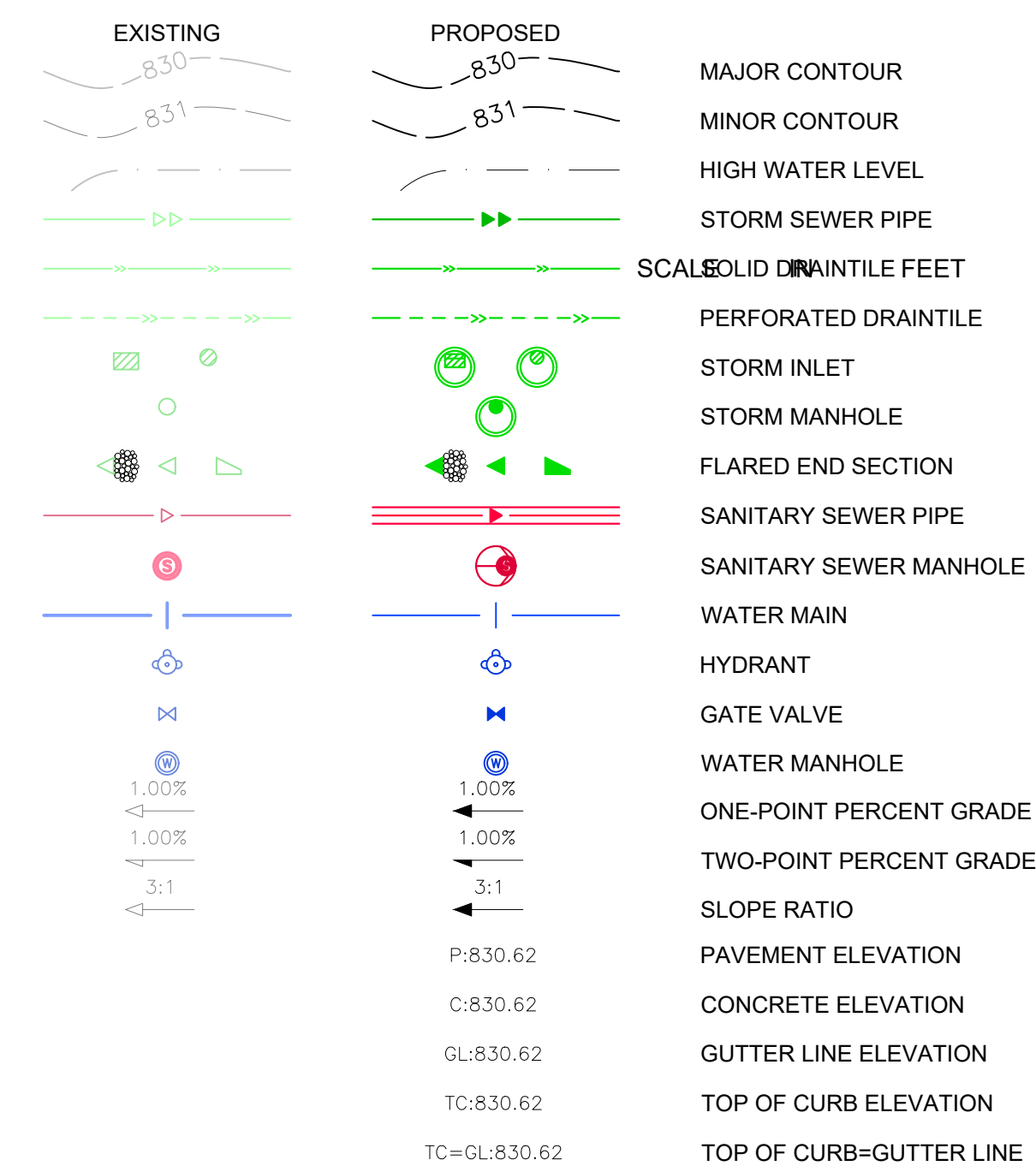
0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.16 FEET. RIM ELEVATIONS SHOWN ON PLANS DO NOT REFLECT SUMPED ELEVATIONS.

19. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 6 INCHES OF TOP SOIL AND SEED/MULCH OR SOD. THESE AREAS SHALL BE WATERED/MAINTAINED BY THE CONTRACTOR UNTIL VEGETATION IS ESTABLISHED.
20. STREETS MUST BE CLEANED AND SWEEPED WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SITES ARE LEFT IDE FOR WEEKENDS AND HOLIDAYS. A REGULAR SWEEPING SCHEDULE MUST BE ESTABLISHED.
21. DUST MUST BE ADEQUATELY CONTROLLED.
22. ON SLOPES 4:1 OR GREATER MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES.
23. ALL STORM DRAINS AND INLETS MUST BE PROTECTED UNTIL ALL SOURCES OF POTENTIAL DISCHARGE ARE STABILIZED.
24. TEMPORARY SOIL STOCKPILES MUST HAVE EFFECTIVE SEDIMENT CONTROL AND CAN NOT BE PLACED IN SURFACE WATERS OR STORM WATER CONVEYANCE SYSTEMS. TEMPORARY STOCKPILES WITHOUT SIGNIFICANT AMOUNTS OF SILT, CLAY, OR ORGANIC COMPOUNDS ARE EXEMPT EX. CLEAN AGGREGATE STOCK PILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES.
25. FINAL STABILIZATION REQUIRES THAT ALL SOIL DISTURBED ACTIVITIES HAVE BEEN COMPLETED AND THAT DISTURBED AREAS ARE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER WITH 70% OF THE EXPECTED FINAL DENSITY; AND THAT ALL PERMANENT PALM TREES HAVE BEEN INSTALLED. ALL TEMPORARY BMPs SHALL BE REMOVED, DITCHES STABILIZED, AND SEDIMENT SHALL BE REMOVED FROM PERMANENT CONVEYANCES AND SEDIMENTATION BASINS IN

ORDER TO RETURN THE POND TO DESIGN CAPACITY

26. THE WATERSHED DISTRICT OR THE CITY MAY HAVE REQUIREMENTS FOR INSPECTIONS VERIFYING PROPER CONSTRUCTION OF THE BMPs.
27. SEE SWPPP FOR ADDITIONAL EROSION CONTROL NOTES AND REQUIREMENTS.
28. SEE UTILITY PLAN FOR WATER, STORM AND SANITARY SEWER INFORMATION.

LEGEND:



PRELIMINARY
NOT FOR
CONSTRUCTION

PROJECT INFORMATION

BLOCK 1 RESIDENTIAL

710 SOUTH 4TH STREET
MINNEAPOLIS, MN 55415

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name

CHAD H. LOCKWOOD

REGISTRATION NO.	DATE
44986	

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DRAWN BY

DRAWN BY TJR
CHECKED CHL

JOB NO.

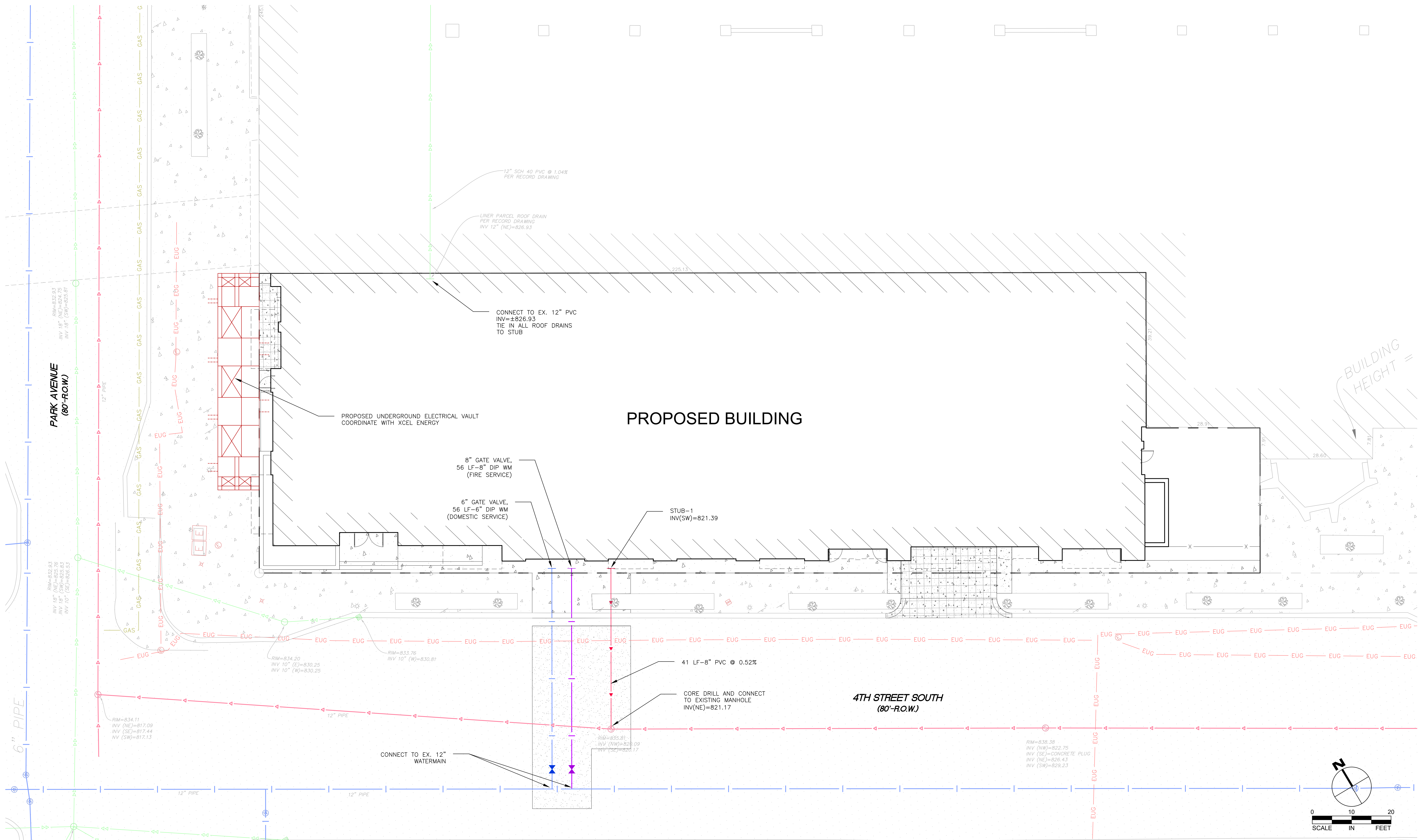
JOB NO. 700-515 **DATE** 12/19/2018

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LUA SUBMITTAL

UTILITY PLAN

C500



UTILITY NOTES

1. PRIOR TO CONSTRUCTION, SUBCONTRACTORS WHOSE WORK REQUIRES EXCAVATION SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND INVERTS, SHOWN OR NOT SHOWN. ANY DISCREPANCY BETWEEN PLANS AND FIELD CONDITIONS SHALL BE REPORTED TO THE OWNER IMMEDIATELY.
 2. THE UTILITY CONTRACTOR SHALL VERIFY THE LOCATIONS AND ELEVATIONS OF THE BUILDING PIPE SYSTEM PRIOR TO CONSTRUCTING SERVICE CONNECTIONS.
 3. ALL UTILITY WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF MINNEAPOLIS SPECIFICATIONS AND BUILDING PERMIT REQUIREMENTS.
 4. UTILITY TRENCHES SHALL BE COMPACTED IN ACCORDANCE WITH GUIDELINES PROVIDED BY GEOTECHNICAL ENGINEER.
 5. CONNECTION TO THE EXISTING SANITARY SEWER AND WATER REQUIRES INSPECTION BY THE CITY OF MINNEAPOLIS.
 6. FIELD ADJUST ALL CASTINGS TO MATCH FINAL GRADES.
 7. THE EXISTING BITUMINOUS PAVEMENT SHALL BE SAWCUT IN ACCORDANCE WITH MINNEAPOLIS UTILITY CONNECTION REQUIREMENTS.
 8. THE UTILITY CONTRACTOR WILL BE RESPONSIBLE FOR OBTAINING THE APPROPRIATE SEWER, WATER AND PLUMBING PERMITS FROM THE CITY'S BUILDING DEPARTMENT.
 9. GENERAL CONTRACTOR SHALL COORDINATE WORK WITH ALL UTILITY COMPANIES.
 10. MINIMUM VERTICAL CLEARANCE BETWEEN SANITARY, STORM AND WATERMAIN TO BE 18". MINIMUM HORIZONTAL CLEARANCE BETWEEN SEWER (SANITARY/STORM) AND WATERMAIN TO BE 10' MINIMUM.
 11. ALL NEW WATERMAIN AND SERVICES MUST HAVE A MINIMUM OF 8.0 FEET OF COVER. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONFLICTS WITH SANITARY SEWER, STORM SEWER, AND SERVICES AS REQUIRED. INSULATION OF WATER AND SANITARY SEWER LINES SHALL BE PROVIDED WHERE 8.0 FEET MINIMUM DEPTH CAN NOT BE ATTAINED.
 12. PROPOSED PIPE MATERIALS:

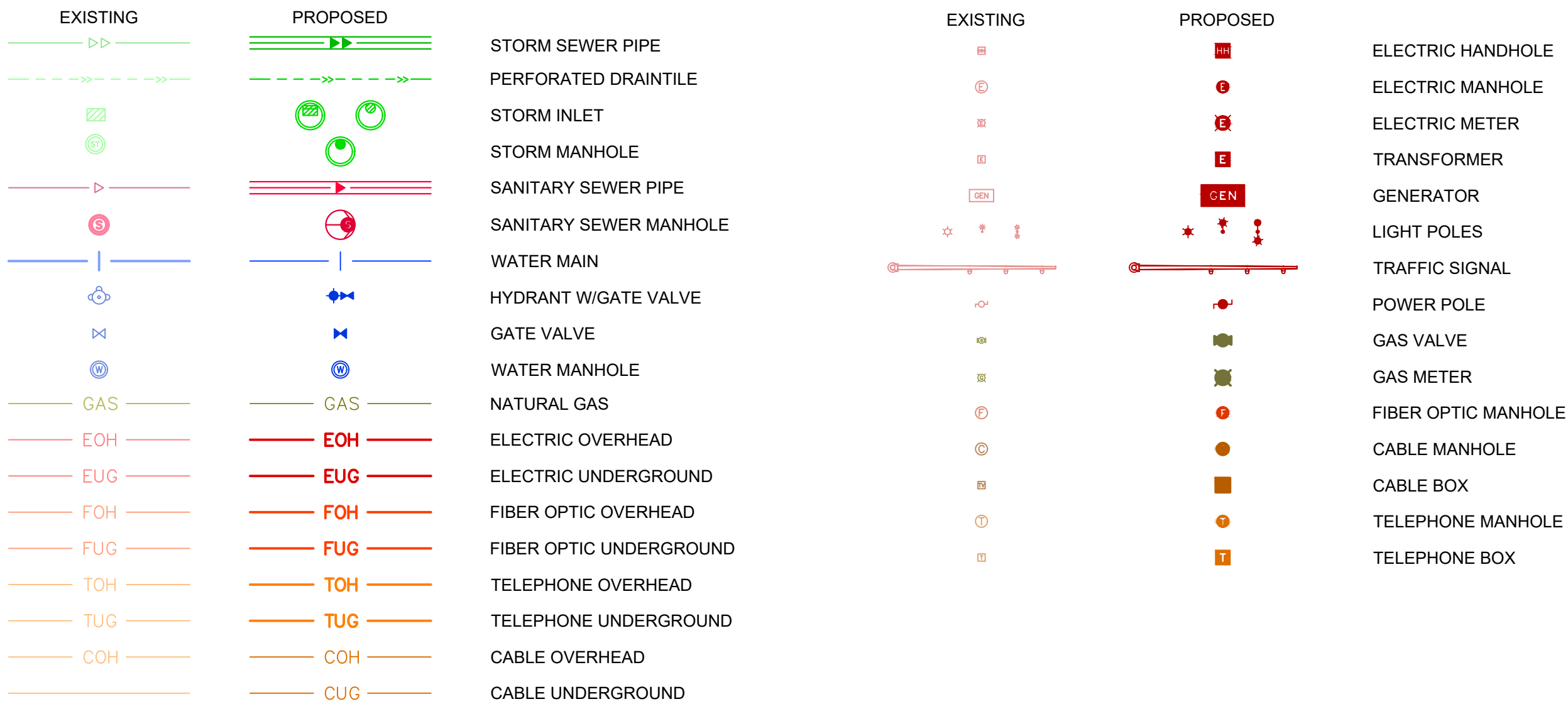
STORM SEWER	DUAL WALL HDPE
STORM SEWER	SCH 40 PVC
WATERMAIN	CLASS 52 DIP
SANITARY SEWER	SCH 40 PVC
 13. ALL PORTIONS OF THE STORM SEWER SYSTEM, INCLUDING CATCH BASINS, LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED ACCORDANCE WITH MINNESOTA RULES, PART 4715.2820.
 14. ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE GASTIGHT OR WATERTIGHT (SEE MINNESOTA RULES, PART 4715.0700). APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, AND OTHER STRUCTURES.
 - a) HIGH-DENSITY POLYETHYLENE (HDPE) STORM DRAINS MUST COMPLY WITH MINNESOTA RULES, PART 4715.0540:

- b) PIPES 4-INCH TO 10-INCH IN SIZE MUST COMPLY WITH AASHTO M252.
- c) PIPES 12-INCH TO 60-INCH IN SIZE MUST COMPLY WITH ASTM F2306.
- d) ALL FITTINGS MUST COMPLY WITH ASTM D3212.
- e) WATER-TIGHT JOINTS MUST BE USED AT ALL CONNECTIONS INCLUDING STRUCTURES.

CITY OF MINNEAPOLIS NOTES

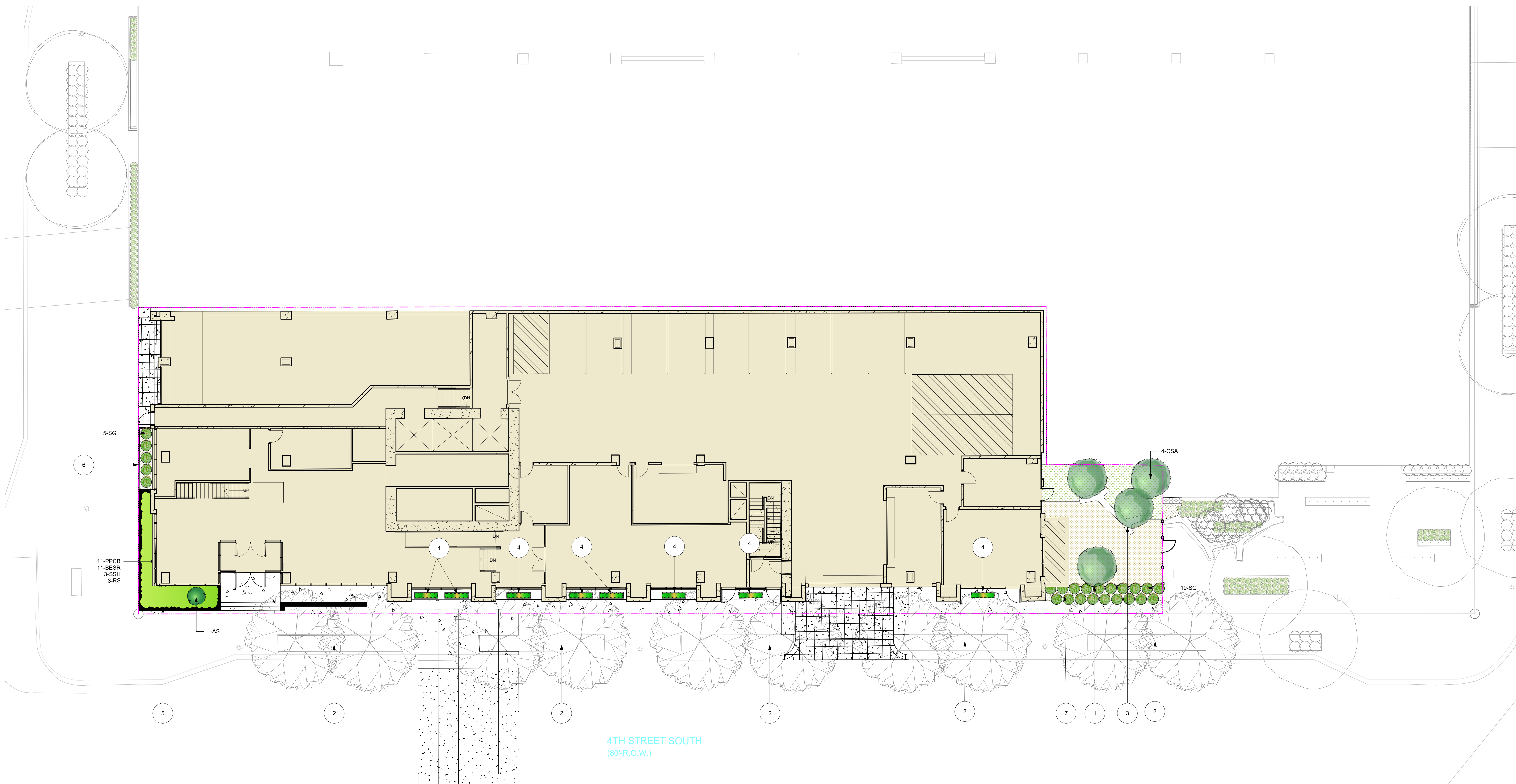
1. THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP (CONTACT PAUL CHELSEN, 612-673-2406 OR PAUL.CHELSEN@MINNEAPOLIS.MN.GOV).
2. UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE DESIGN OF THE STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.


LEGEND:




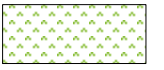


ISSUE RECORD

ISSUE #	DATE	DESCRIPTION
A	11/21/18	PDR SUBMITTAL
B	12/19/2018	LAND USE SUBMITTAL



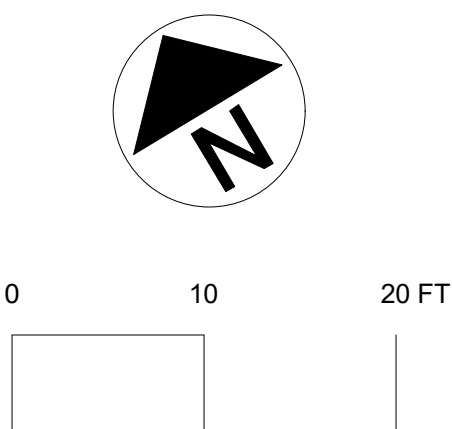
PLANT LIST				
Common Name	Botanical Name	Qty	Size	
Deciduous Trees				
 COLUMNAR SWEDISH ASPEN	Populus tremula 'Erecta'	4	20 GAL	
Shrubs				
AS	ACROCONA SPRUCE	1	7 GAL	
BESR	BLACK-EYED SUSAN RUDBECKIA	11	1 GAL	
PPCB	PLUM PUDDING CORAL BELLS	11	1 GAL	
RS	RUSSIAN SAGE	3	1 GAL	
SSH	SUM AND SUBSTANCE HOSTA	3	1 GAL	
		0		
Ornamental Grasses				
SG	SWITCH GRASS	19	1 GAL	

LEGEND

	4" HARDWOOD MULCH WITH IRRIGATION
	SYNTHETIC TURF WITH IRRIGATION
	TRAP ROCK MULCH
	PEA GRAVEL WITH UNDERLAYMENT AND IRRIGATION

KEY NOTES

- DOG RUN WITH 5' HT ORNAMENTAL FENCING
- EXISTING STREET SIDE GARDENS WITH TREES/PERENNIALS PROTECTION FROM CONSTRUCTION
- (2) LANDSCAPE FORMS - TWIGS BENCHES
- TOURN SOL 18" X 72" WILSHIRE ANNUAL PLANTERS
- RAISED PLANTER
- 4" CURBED PLANTING AREA
- AT GRADE PLANTING AREA



PRELIMINARY
NOT FOR
CONSTRUCTION

PROJECT INFORMATION
**BLOCK 1
RESIDENTIAL**

LOCATION
700 4TH STREET
MINNEAPOLIS, MN 55415

I hereby certify that this plan, specification,
or report was prepared by me or under my
direct supervision and that I am a duly
Licensed Professional Landscape Architect under the
laws of the State of Minnesota.

REGISTRATION NO. DATE

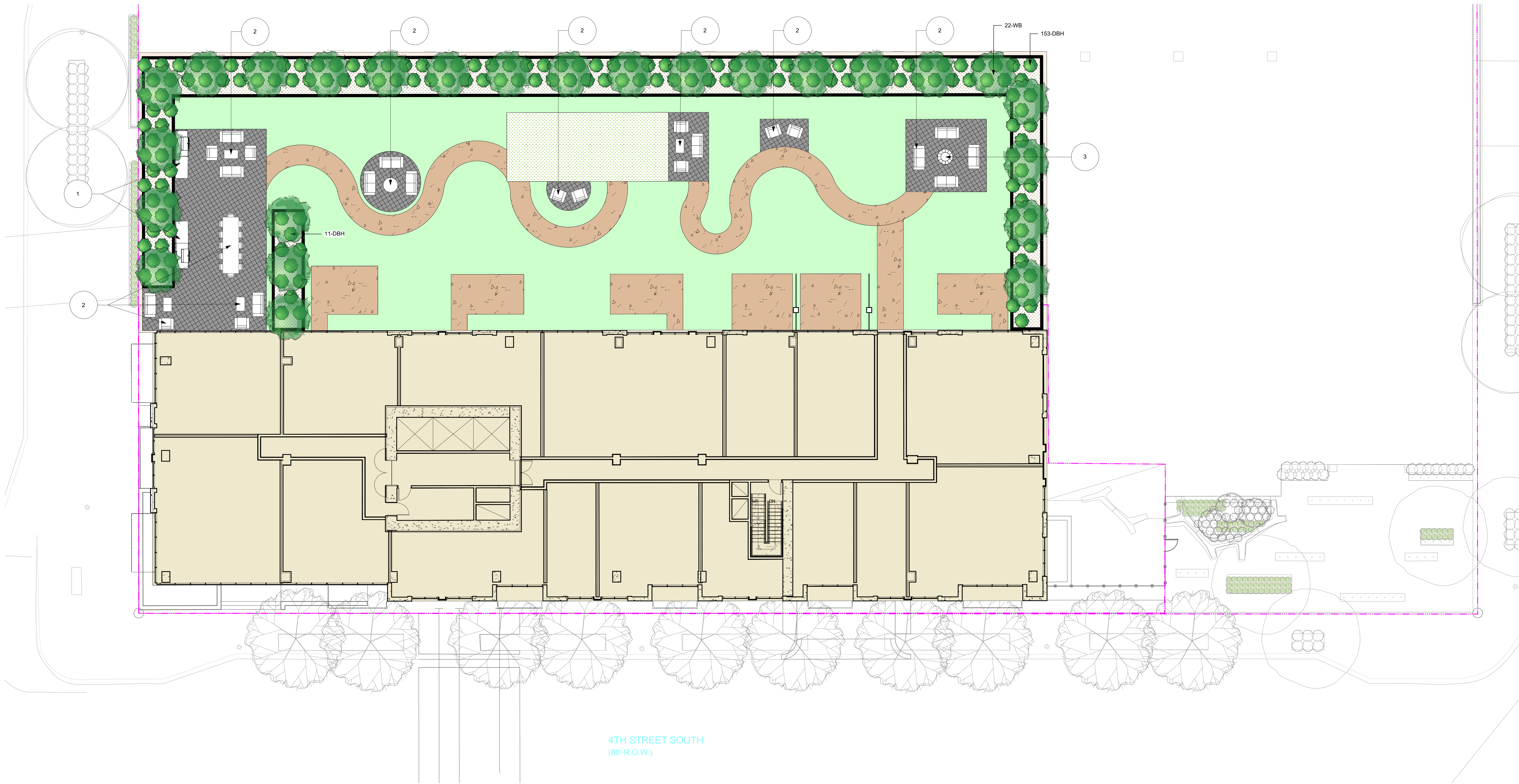
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CLR JC
JOB NO. DATE
700-515 11/21/2018

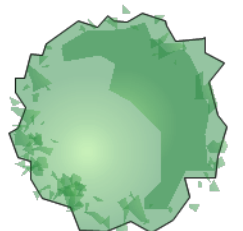
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION
A	11/21/18	PDR SUBMITTAL
B	12/19/2018	LAND USE SUBMITTAL
C	2/1/2019	LAND USE RESUBMITTAL

LAND USE
RESUBMITTAL
2/1/2019

6TH FLOOR
AMENITY DECK

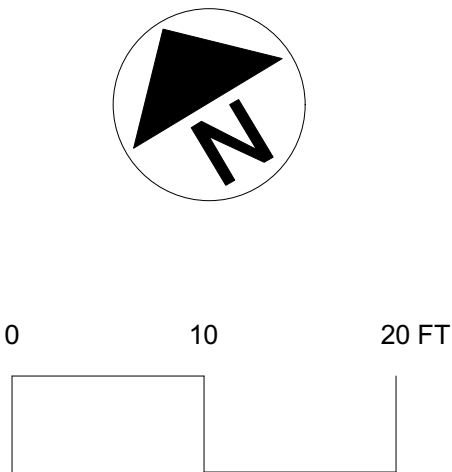
L200



	PLANT LIST			
	Common Name	Botanical Name	Qty	Size
	Deciduous Trees			
	Whitespire Birch	Betula populifolia 'Whitespire'	25	12' CLMP B&B
	Shrubs			
DBH	DWARF BUSH HONEYSUCKLE	Diervilla lonicera	164	5 GAL

- LEGEND**
- NON-OCCUPIABLE/ RESIDENTIAL PEDESTAL PAVER
 - OCCUPIABLE PEDESTAL PAVERS
 - ARTIFICIAL TURF
 - 3" CIP RAISED PLANTERS WITH HARDWOOD MULCH
 - 6" SEDUM PLANTING BEDS WITH NATIVES
 - 6' HT SCREEN FENCING

- KEY NOTES**
- 1 2 GRILL STATIONS
 - 2 MOVABLE FURNITURE (NIC)
 - 3 FIREPIT



GENERAL NOTES

GENERAL:
LANDSCAPE CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK

LANDSCAPE CONTRACTOR SHALL VERIFY PLAN LAYOUT AND DIMENSIONS SHOWN AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN AND/OR INTENT OF THE PROJECTS LAYOUT.

LANDSCAPE CONTRACTOR SHALL ASSURE COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK AND/OR MATERIALS SUPPLIED.

LANDSCAPE CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION OPERATIONS. DAMAGE TO SAME SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.

LANDSCAPE CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.

LANDSCAPE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS (UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES) ON PROPERTY WITH THE GENERAL CONTRACTOR AND BY CALLING GOPHER STATE ONE CALL (651-454-0002) PRIOR TO STAKING PLANT LOCATIONS. LANDSCAPE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE IF DAMAGED DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE OWNER.

EXISTING CONTOURS, TRAILS, VEGETATION, CURB/GUTTER AND OTHER ELEMENTS ARE BASED UPON INFORMATION SUPPLIED TO THE LANDSCAPE ARCHITECT BY OTHERS. LANDSCAPE CONTRACTOR SHALL VERIFY DISCREPANCIES PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT OF SAME.

EXISTING TREES TO REMAIN SHALL BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD, DAMAGED AND/OR RUBBING BRANCHES BY OTHERS.

SITE:
UNDERGROUND UTILITIES SHALL BE INSTALLED SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF ANY EXISTING TREES TO REMAIN.

CONTRACTOR SHALL CONSTRUCT DRAINAGE BASINS AS NEEDED PER STATE OF MINNESOTA SPECS.

CONTRACTOR SHALL PROVIDE TEMPORARY COVERING FOR CATCH BASINS AND MAN HOLES UNTIL FINISHED GRADING IS COMPLETE.

ADD EROSION CONTROL MEASURES IF GRADES GREATER THAN 3:1 OR IF CONDITIONS WARRANT. REFER TO STATE OF MINNESOTA REQUIREMENTS FOR EROSION CONTROL MEASURES.

CONTRACTOR SHALL MAINTAIN EROSION CONTROL MEASURES, INCLUDING THE REMOVAL OF ACCUMULATED SILT IN FRONT OF SILT FENCES AND EXCESS SEDIMENT IN PROPOSED CATCH BASINS, FOR THE DURATION OF CONSTRUCTION TO INSURE THE MINIMIZATION OF EROSION.

WITHIN TWO WEEKS OF FINISHED SITE GRADING, DISTURBED AREAS SHALL BE STABILIZED WITH SEED, SOD OR ROCK BASE.

LANDSCAPE CONTRACTOR WILL REMOVE EROSION CONTROL MEASURES AFTER VEGETATION IS ESTABLISHED.

ROUGH GRADING AND FINISHED GRADING TO BE DONE BY EARTHWORK CONTRACTOR.

LANDSCAPE CONTRACTOR SHALL INSURE THAT SOIL CONDITIONS AND COMPACTION ARE ADEQUATE TO ALLOW FOR PROPER DRAINAGE AROUND THE CONSTRUCTION SITE. UNDESIRABLE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK. IT SHALL BE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY TO INSURE PROPER SURFACE AND SUBSURFACE DRAINAGE IN ALL PLANTING AREAS.

SALVAGE TOPSOIL FROM THE EARTHWORK AREAS AS APPROPRIATE AND/OR AS DIRECTED BY LANDSCAPE ARCHITECT AND STOCKPILE FOR REUSE IN LOCATION APPROVED BY OWNER.

LANDSCAPE CONTRACTOR SHALL VERIFY A 1% MINIMUM GRADE AWAY FROM BUILDINGS WITHIN LIMITS OF CONSTRUCTION. SUCH CONTRACTOR IS NOT TO ACCEPT SITE AS READY FOR LANDSCAPE INSTALLATION UNTIL GRADES ARE ESTABLISHED WITH TOPSOIL IN PLACE.

WHERE SOD ABUTS PAVED SURFACES, FINISHED GRADE OF SOD SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB OR CURB.

MAINTENANCE STRIPS, WHERE ILLUSTRATED, TO HAVE EDGER AND STONE MULCHES INDICATED ON DRAWINGS OR IN SPECS.

PLANTINGS:
NO PLANTS WILL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.

PROPOSED PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, ANSI Z60.1. UNLESS NOTED OTHERWISE, DECIDUOUS SHRUBS SHALL HAVE AT LEAST 5 CANES AT THE SPECIFIED HEIGHT. ORNAMENTAL TREES SHALL HAVE NO V CROTCHES AND SHALL BEGIN BRANCHING NO LOWER THAN 3' FEET ABOVE THE ROOT BALL. STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE FINISHED GRADE.

PLAN TAKES PRECEDENCE OVER PLANT SCHEDULE IF DISCREPANCIES IN QUANTITIES EXIST. SPECIFICATIONS AND DETAILS TAKE PRECEDENCE OVER NOTES.

PLANT MATERIAL SHALL BE LOCATED AND STAKED FOR LANDSCAPE ARCHITECT TO APPROVE. ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS MAY BE NEEDED IN FIELD. LANDSCAPE ARCHITECT MUST BE NOTIFIED PRIOR TO ADJUSTMENT OF PLANTS. PLANTINGS SHALL BE PLANTED A MINIMUM OF 3' AWAY FOR HARDSCAPING AND BUILDINGS.

NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS REQUESTED OF THE LANDSCAPE ARCHITECT BY THE LANDSCAPE CONTRACTOR PRIOR TO THE SUBMISSION OF BID AND/OR QUOTATION.

PLANT MATERIALS TO BE INSTALLED PER PLANTING DETAILS.

CONTRACTOR SHALL REMOVE WIRING, TWINE/ROPES AND BURLAP FROM THE TOP AND SIDES OF THE ROOT BALL OF BALLED AND BURLAPPED PLANTS.

CONTRACTOR SHALL REMOVE CONTAINERS AND CUT CIRCLING ROOTS IF PLANTS ARE CONTAINER GROWN.

PLANTING AREAS RECEIVING GROUNDCOVERS, PERENNIALS, ANNUALS, AND/OR VINES SHALL RECEIVE A MINIMUM OF 12" DEPTH OF PLANTING SOIL CONSISTING OF AT LEAST 45 PARTS TOPSOIL, 45 PARTS SCREENED COMPOST OR MANURE AND 10 PARTS SAND.

PLANTING AREAS RECEIVING SOD/SEED SHALL RECEIVE A MINIMUM OF 6" DEPTH OF PLANTING SOIL CONSISTING OF AT LEAST 45 PARTS TOPSOIL, 45 PARTS SCREENED COMPOSE OR MANURE AND 10 PARTS SAND.

PLANTING AREAS RECEIVING SHRUBS/TREES SHALL RECEIVE A MINIMUM OF 18" DEPTH OF PLANTING SOIL CONSISTING OF AT LEAST 45 PARTS TOPSOIL, 45 PARTS SCREENED COMPOSE OR MANURE AND 10 PARTS SAND.

UNLESS NOTED OTHERWISE, THE APPROPRIATE DATES FOR SPRING PLANT MATERIAL INSTALLATION AND SOD PLACEMENT IS FROM THE TIME GROUND HAS THAWED TO JUNE 15.

PLANTING AND SODDING IN THE FALL IS GENERALLY ACCEPTABLE TO NOVEMBER 1 OR EARLIER IF HEAVY FROST.

CONIFEROUS PLANTING IS GENERALLY ACCEPTABLE FROM AUGUST 15 TO OCTOBER 1. FALL DECIDUOUS PLANTING IS GENERALLY ACCEPTABLE FROM THE FIRST FROST UNTIL NOVEMBER 1. ADJUSTMENTS TO PLANTING DATES MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.

PLANT MATERIAL SHALL BE FERTILIZED UPON INSTALLATION WITH APPROVED FERTILIZER MIXED IN WITH THE PLANTING SOIL MIX PER THE MANUFACTURER'S INSTRUCTIONS OR MAY BE TREATED FOR SUMMER AND FALL INSTALLATION WITH AN APPLICATION OF GRANULAR 0-20 OF 12 OZ. PER 2.5" CALIPER TREE AND 6 OZ. PER SHRUB WITH AND ADDITIONAL APPLICATION OF 10-10-10 THE FOLLOWING SPRING IN THE TREE SAUCER.

TREE STAKING: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO STAKE AND/OR GUY THE TREES ACCORDING TO THE DETAILS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE EVERY STEP NECESSARY TO MAINTAIN THE TREES AND SHRUBS IN AN UPRIGHT AND PLUMB CONDITION AT ALL TIMES UNTIL THE END OF THE PLANT GUARANTEE PERIOD ESPECIALLY WHERE VANDALISM, SOIL OR WIND CONDITIONS ARE A PROBLEM.

WRAPPING MATERIAL SHALL BE CORRUGATED POLYETHYLENE PIPING 1" GREATER IN CALIPER THAN THE TREE BEING PROTECTED OR QUALITY, HEAVY, WATERPROOF CREPE PAPER MANUFACTURED FOR THIS PURPOSE. WRAP DECIDUOUS TREES PLANTED IN THE FALL PRIOR TO DECEMBER 1 AND REMOVE WRAPPING AFTER MAY 1.

ALL PLANT MATERIAL SHALL BE THOROUGHLY WATERED TWICE DURING THE FIRST 24-HOUR PERIOD AFTER INSTALLATION. THEN ALL PLANTINGS SHALL BE WATERED A MINIMUM OF BI-WEEKLY, OR MORE IF NECESSARY, FOR THE FIRST 2 WEEKS THEN AS NECESSARY DURING THE FIRST GROWING SEASON.

EDGING SHALL BE STRAIGHT EDGED COMMERCIAL GRADE EDGING (REFER TO SPECS) AND TO BE USED TO CONTAIN SHRUBS, PERENNIALS AND ANNUALS WHERE PLANTING BED MEETS SOD UNLESS NOTED OTHERWISE.

ANNUAL AND PERENNIAL PLANTING BEDS TO RECEIVE 3" DEEP SHREDDED HARDWOOD MULCH WITH NO WEED BARRIER.

SHRUB MASSING BEDS TO RECEIVE 4" DEEP SHREDDED HARDWOOD MULCH UNLESS NOTED OTHERWISE.

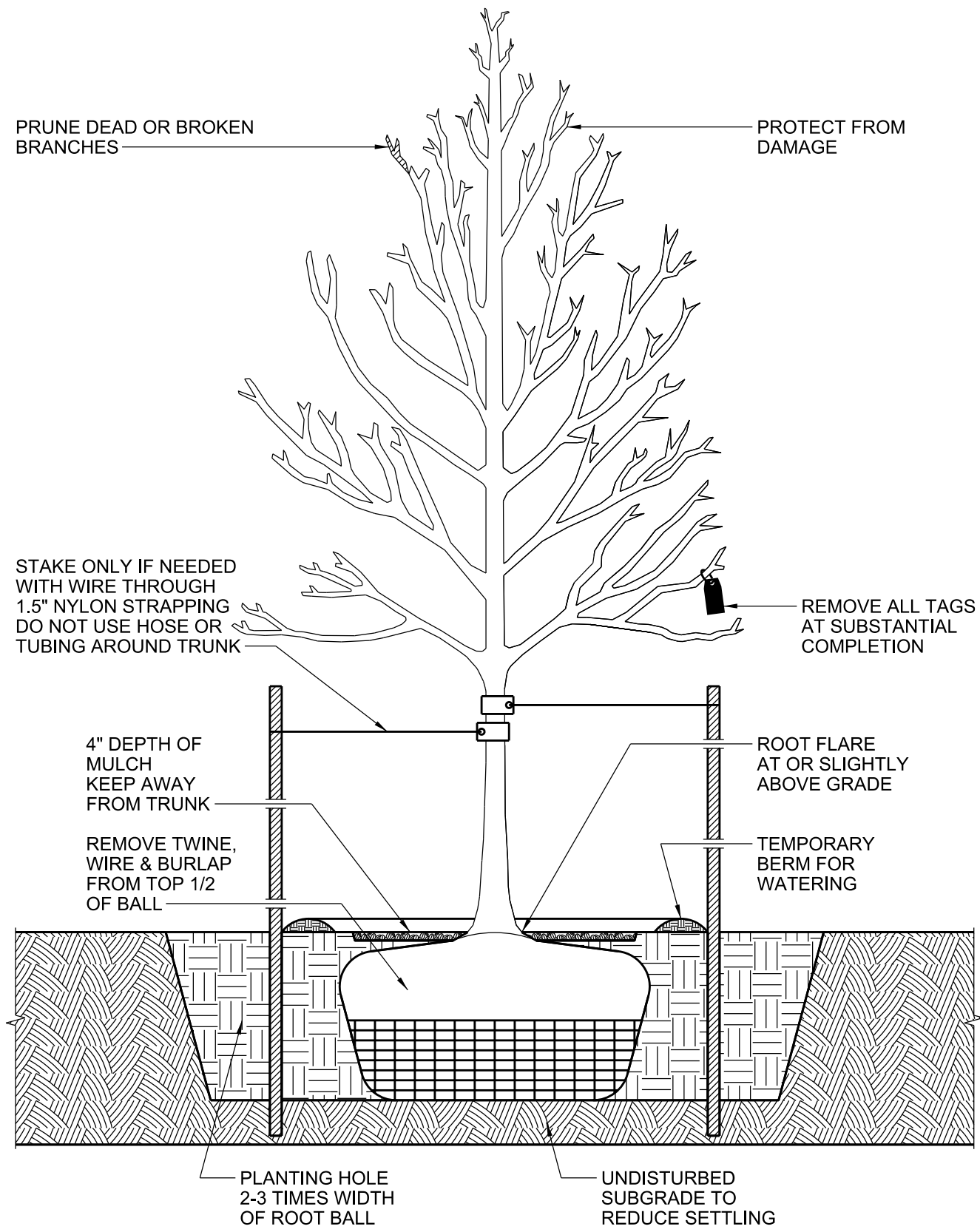
TREES TO RECEIVE 4" DEEP SHREDDED HARDWOOD MULCH AND SHALL HAVE NO MULCH IN DIRECT CONTACT WITH THE TREE TRUNK.

ALL PARKING LOT ISLANDS TO RECEIVE 4" DEEP SHREDDED HARDWOOD MULCH UNLESS OTHERWISE NOTED.

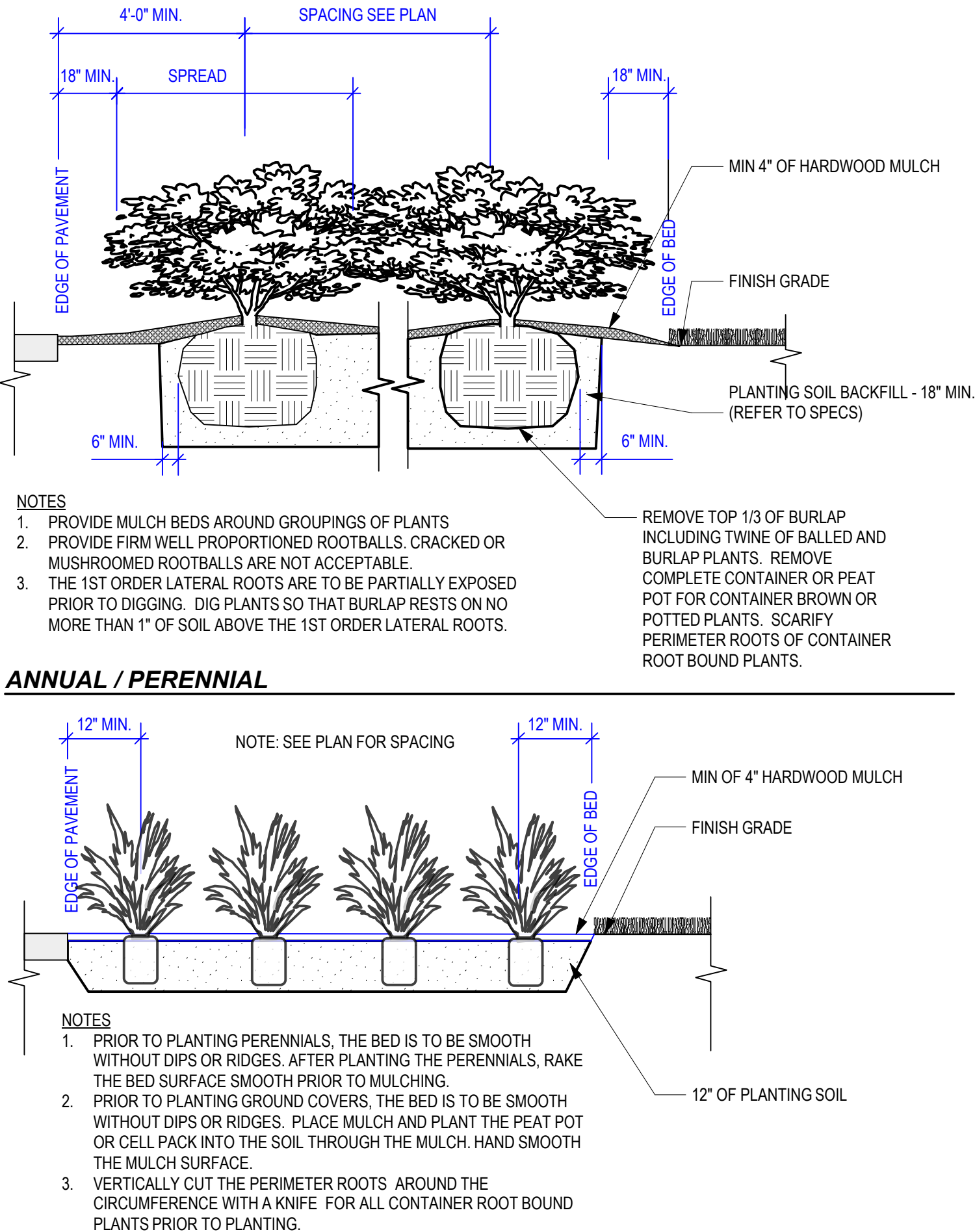
SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, SOD SHALL BE STAKED SECURELY.

THE APPLICANT IS REQUIRED TO MANAGE THE SEEDED PRAIRIE PER RECOMMENDATIONS OF THE LATEST MN/DOT SEEDING MANUAL OR QUALIFIED PRAIRIE MANAGEMENT PERSONNEL. THE APPLICANT IS ENCOURAGED TO HIRE A QUALIFIED COMPANY TO DO THIS DURING ESTABLISHMENT PERIOD. THE NATURAL RESOURCES DEPT. MUST BE PROVIDED A MANAGEMENT PLAN FOR THE FIRST 3 YEARS AFTER SEEDING BEFORE LANDSCAPING WARRANTY BEGINS.

DECIDUOUS TREE



SHRUB



IRRIGATION:
LANDSCAPE CONTRACTOR SHALL WATER PLANTINGS THROUGH PLANT ESTABLISHMENT PERIOD UNLESS AGREED UPON OTHERWISE.

ALL LANDSCAPED AREAS SHALL BE IRRIGATED WITH RAIN SENSORS, UNLESS OTHERWISE NOTED. LANDSCAPE CONTRACTOR SHALL SUBMIT A SHOP DRAWING OF IRRIGATION PLAN TO THE LANDSCAPE ARCHITECT FOR REVIEW AND APPROVAL.

LANDSCAPE CONTRACTOR SHALL VERIFY EXISTING IRRIGATION SYSTEM LAYOUT AND CONFIRM COMPLETE LIMITS OF IRRIGATION.

LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AN IRRIGATION PLAN LAYOUT AND SPECS, AND THE EFFECTIVENESS OF THE NEWLY INSTALLED IRRIGATION SYSTEM.

ACCEPTANCE:
LANDSCAPE CONTRACTOR SHALL WARRANTY NEW PLANT MATERIAL THROUGH TWO CALENDAR YEARS FROM THE DATE OF THE OWNER AND CITY ACCEPTANCE. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.

LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR ONGOING MAINTENANCE OF NEWLY INSTALLED MATERIALS UNTIL TIME OF OWNER ACCEPTANCE. ACTS OF VANDALISM OR DAMAGE WHICH MAY OCCUR PRIOR TO OWNER ACCEPTANCE SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.

LANDSCAPE CONTRACTOR SHALL PREPARE AND SUBMIT A WRITTEN REQUEST FOR THE OWNER ACCEPTANCE INSPECTION OF LANDSCAPE AND SITE IMPROVEMENTS PRIOR TO SUBMITTING FINAL PAY REQUEST.

LANDSCAPE CONTRACTOR SHALL PREPARE AND SUBMIT REPRODUCIBLE AS-BUILT DRAWING(S) OF LANDSCAPE INSTALLATION, SITE IMPROVEMENTS UPON COMPLETION OF CONSTRUCTION INSTALLATION AND PRIOR TO PROJECT ACCEPTANCE.



RYAN A+E, INC.
533 South Third Street, Suite 100
Minneapolis, MN 55415
612-492-4000 tel
612-492-3000 fax

WWW.RYANCOMPANIES.COM

OWNER

CONSULTANTS

PRELIMINARY
NOT FOR
CONSTRUCTION

PROJECT INFORMATION

BLOCK 1
RESIDENTIAL

LOCATION

710 4TH STREET
MINNEAPOLIS, MN 55415

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Landscape Architect under the laws of the State of Minnesota.

REGISTRATION NO. DATE

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DRAWN BY CHECKED BY
CLR JC
JOB NO. DATE
700-515 11/21/2018

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION
A	11/21/18	PDR SUBMITTAL
B	12/19/2018	LAND USE SUBMITTAL

LAND USE
SUBMITTAL

12/19/2018

LANDSCAPE
DETAIL SHEET

L500

**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 12/19/2018

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

**ARCHITECTURAL
SITE PLAN**

A050



**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

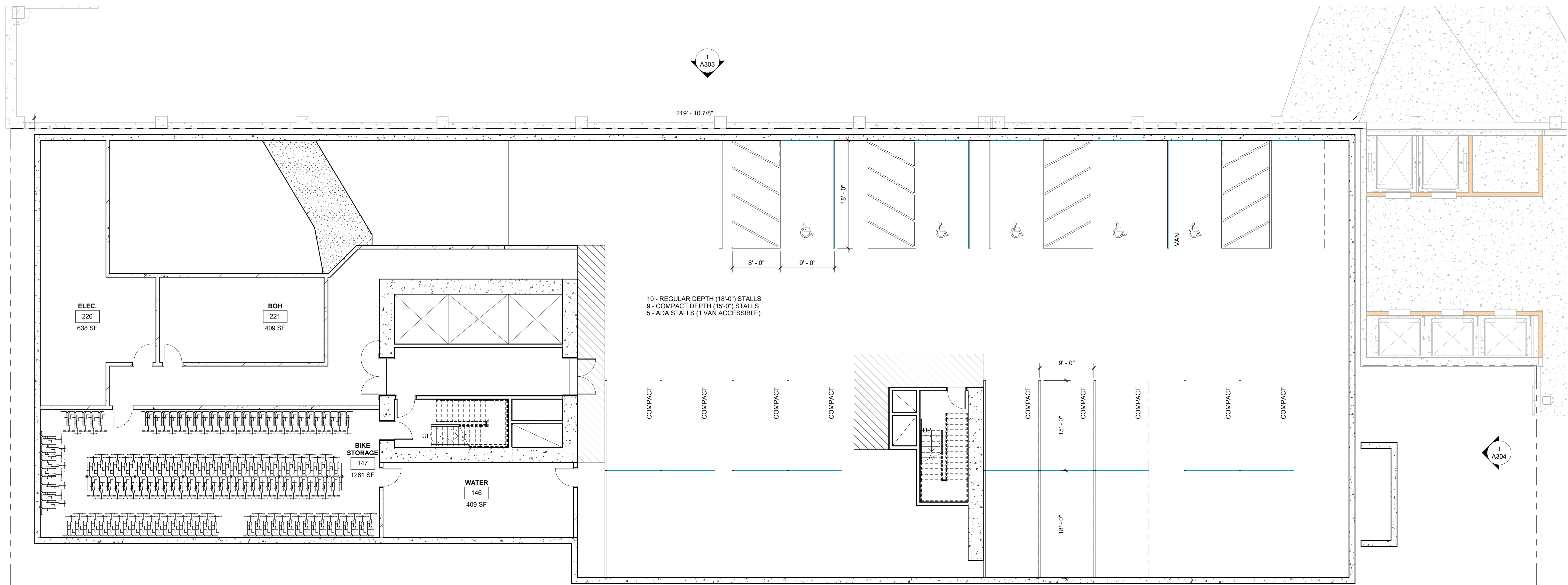
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

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LUA SUBMITTAL
DECEMBER 19TH, 2018

**LOWER LEVEL
PLAN**

A100



BLOCK ONE RESIDENTIAL

710 S 4TH STREET
MINNEAPOLIS MN,
55415

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 00-515	DATE 12/19/2018

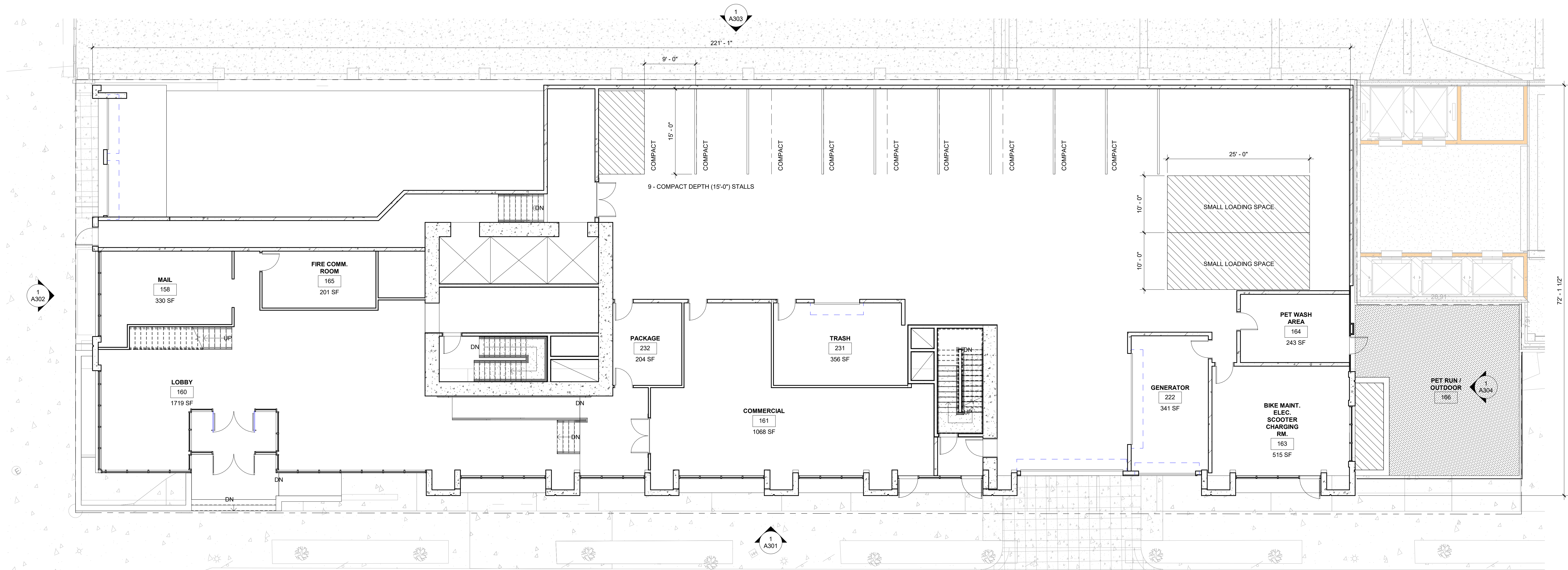
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

LEVEL 1 FLOOR PLAN

A101



**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

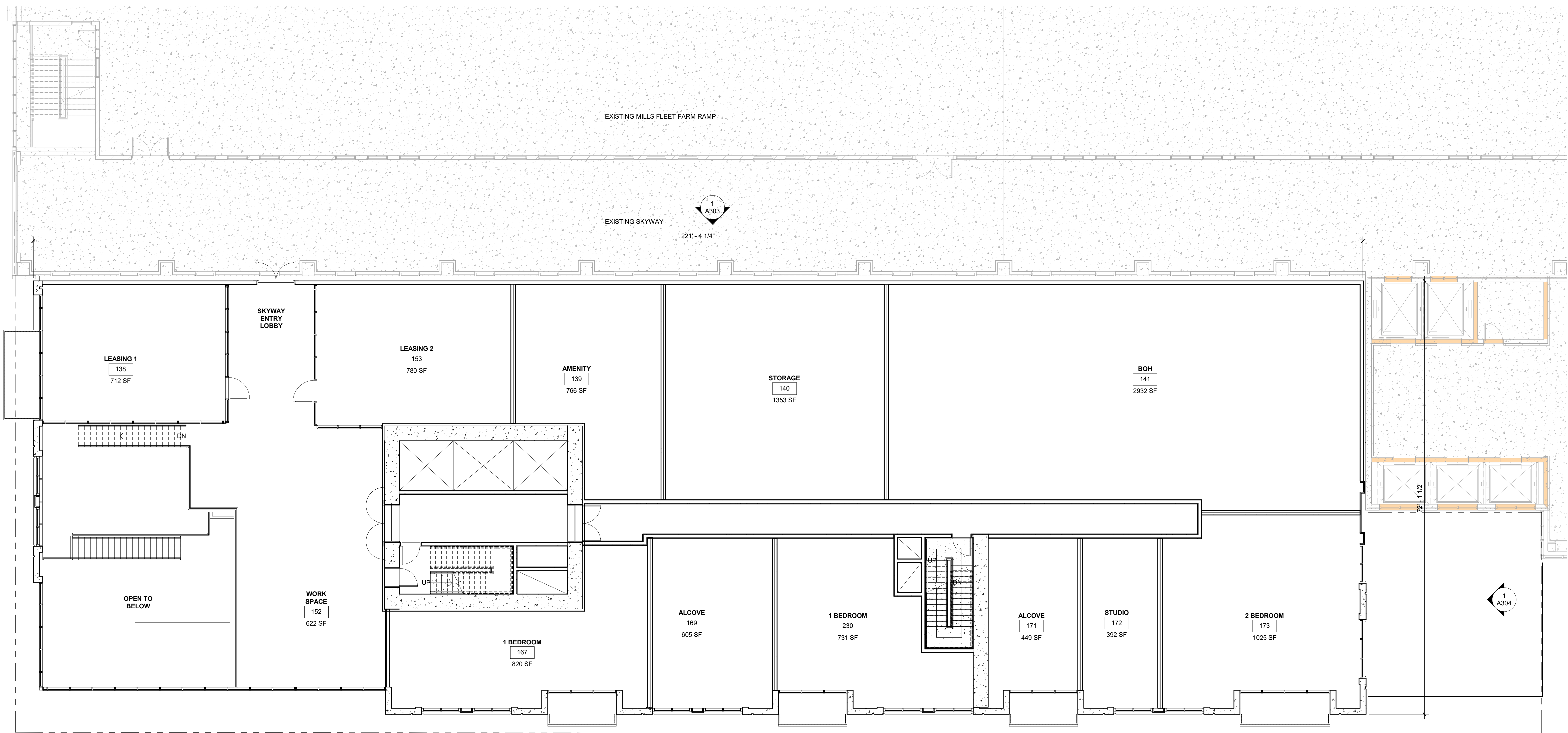
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

**LEVEL 2 FLOOR
PLAN**

A102



**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

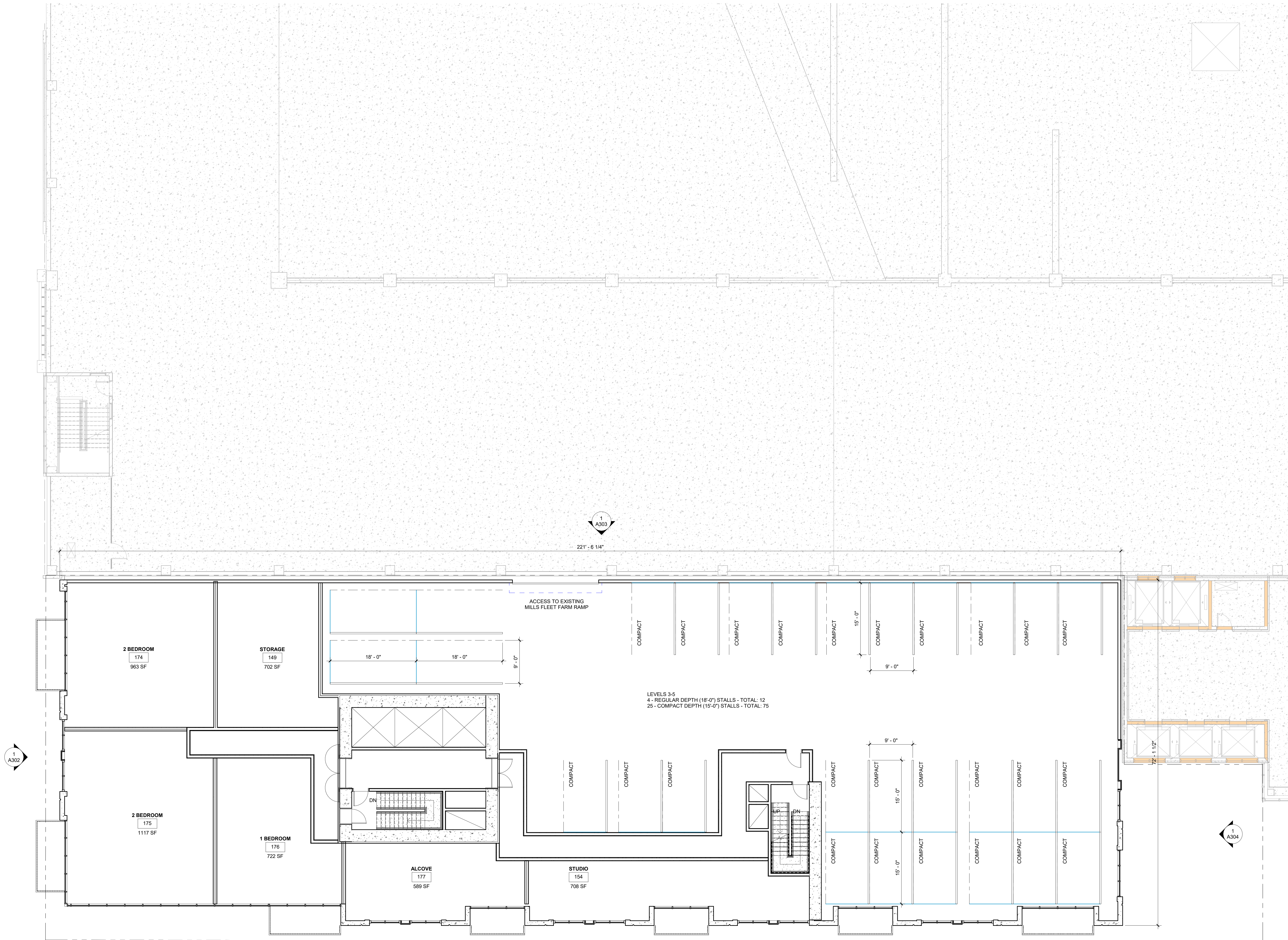
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

**LEVEL 3-5
FLOOR PLAN**

A103



**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 12/19/2018

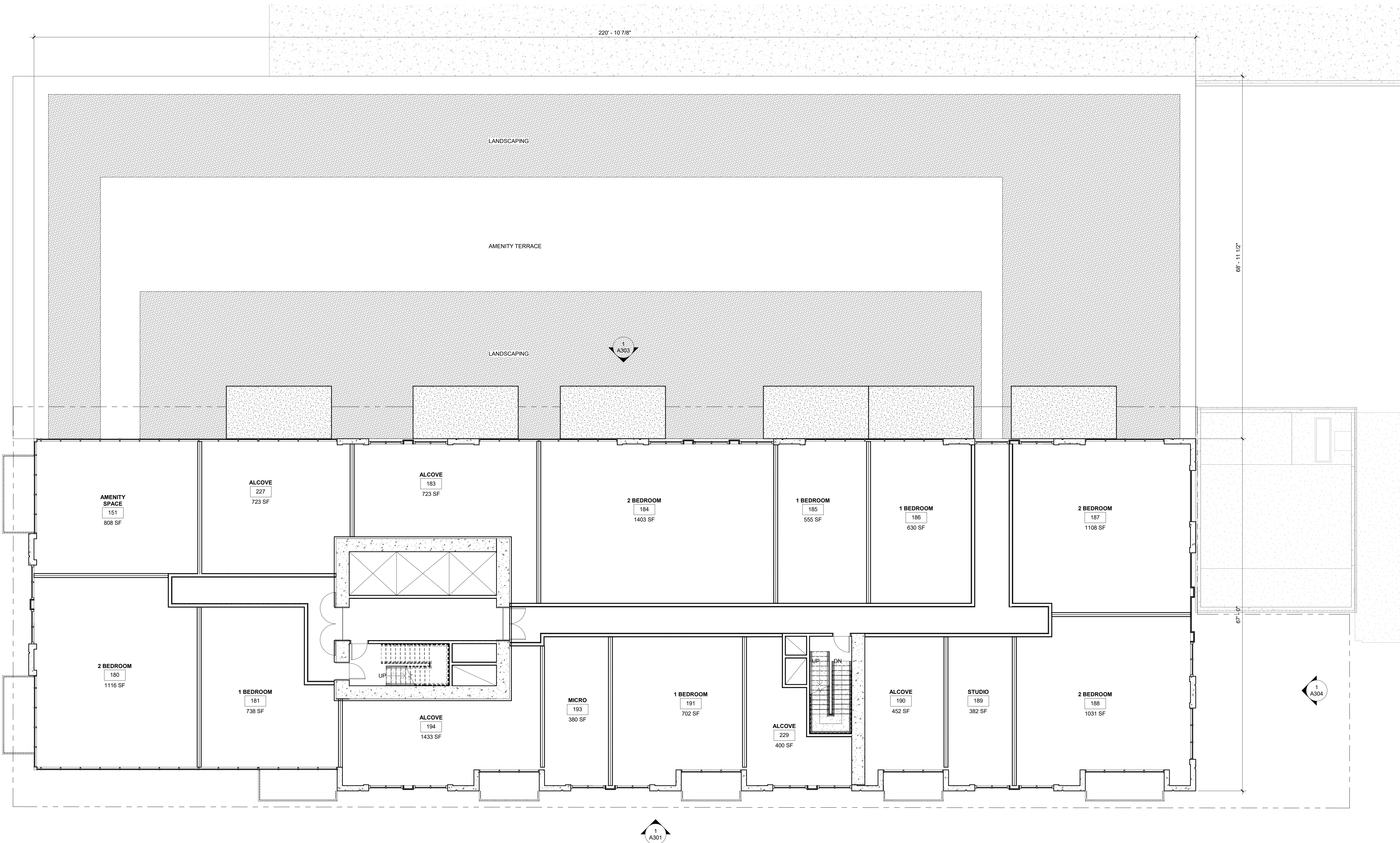
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

**LEVEL 6 FLOOR
PLAN**

A106



**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 12/19/2018

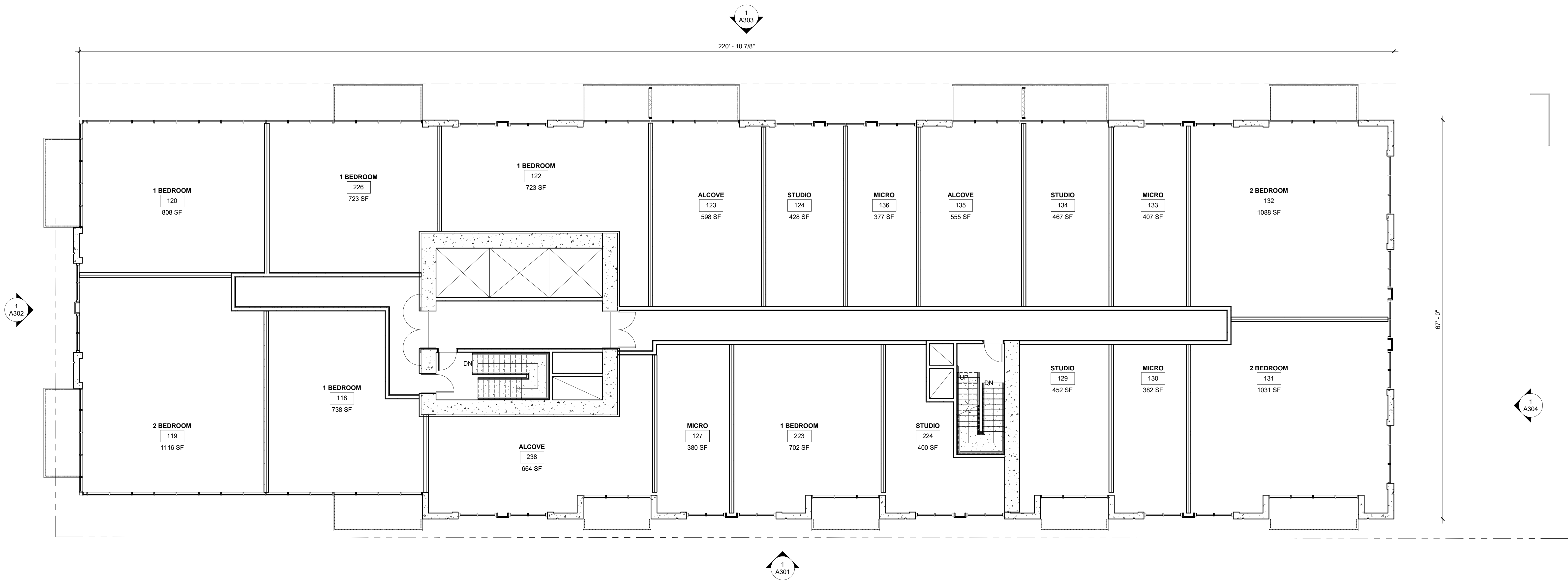
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

**LEVEL 7-21
FLOOR PLAN**

A107





RYAN A+E, INC.
533 South Third Street, Suite 100
Minneapolis, MN 55415
612-492-4000 tel
612-492-3000 fax

WWW.RYANCOMPANIES.COM

OWNER

EXCELSIOR
The Excelsior Group

CONSULTANTS

KEY PLAN

PROJECT INFORMATION

BLOCK ONE RESIDENTIAL

710 S 4TH STREET
MINNEAPOLIS MN,
55415

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DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 12/19/2018

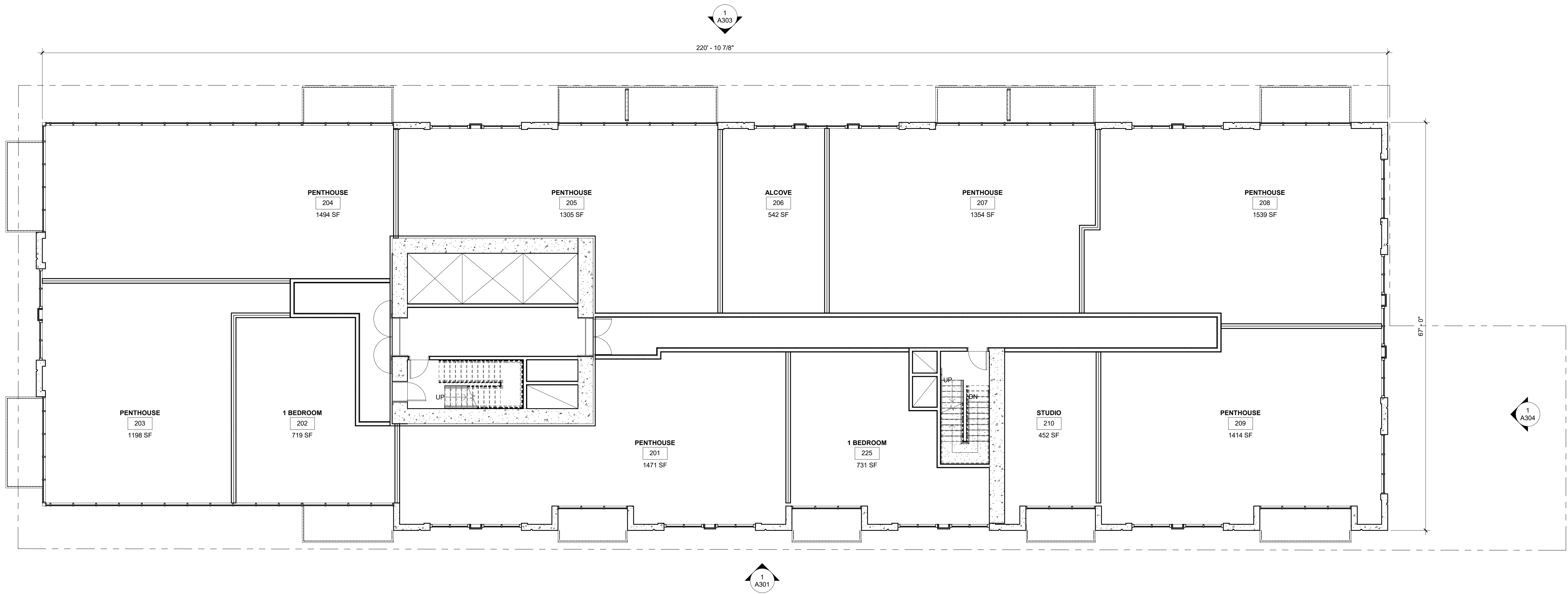
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

LEVEL 22 -
PENTHOUSE
SUITES

A122





RYAN A+E, INC.
533 South Third Street, Suite 100
Minneapolis, MN 55415
612-492-4000 tel
612-492-3000 fax

WWW.RYANCOMPANIES.COM

OWNER

EXCELSIOR
The Excelsior Group

CONSULTANTS

KEY PLAN

PROJECT INFORMATION

BLOCK ONE RESIDENTIAL

710 S 4TH STREET
MINNEAPOLIS MN,
55415

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JOB NO.	DATE
700-515	12/19/2018

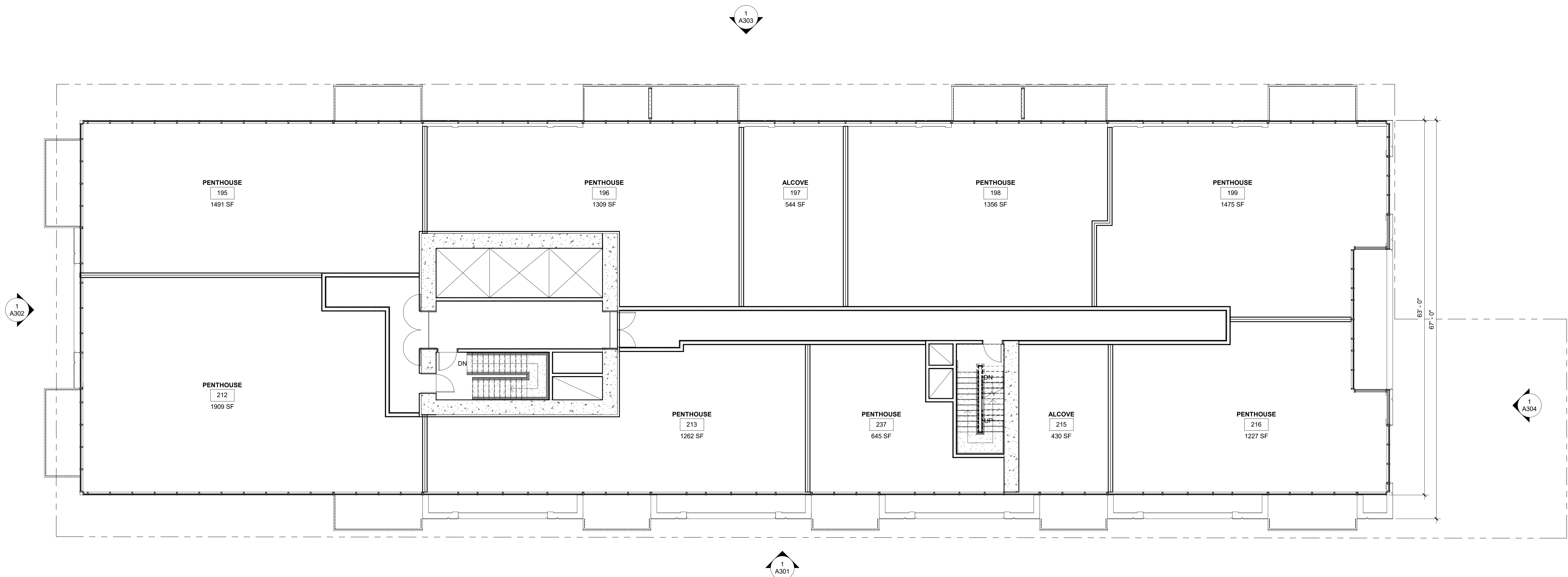
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

LEVEL 23 -
PENTHOUSE
SUITES

A123





RYAN A+E, INC.
533 South Third Street, Suite 100
Minneapolis, MN 55415
612-492-4000 tel
612-492-3000 fax

WWW.RYANCOMPANIES.COM

OWNER

EXCELSIOR
The Excelsior Group

CONSULTANTS

KEY PLAN

PROJECT INFORMATION

BLOCK ONE
RESIDENTIAL

710 S 4TH STREET
MINNEAPOLIS MN,
55415

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DRAWN BY	CHECKED BY
CRC	JRE
JOB NO.	DATE
700-515	12/19/2018

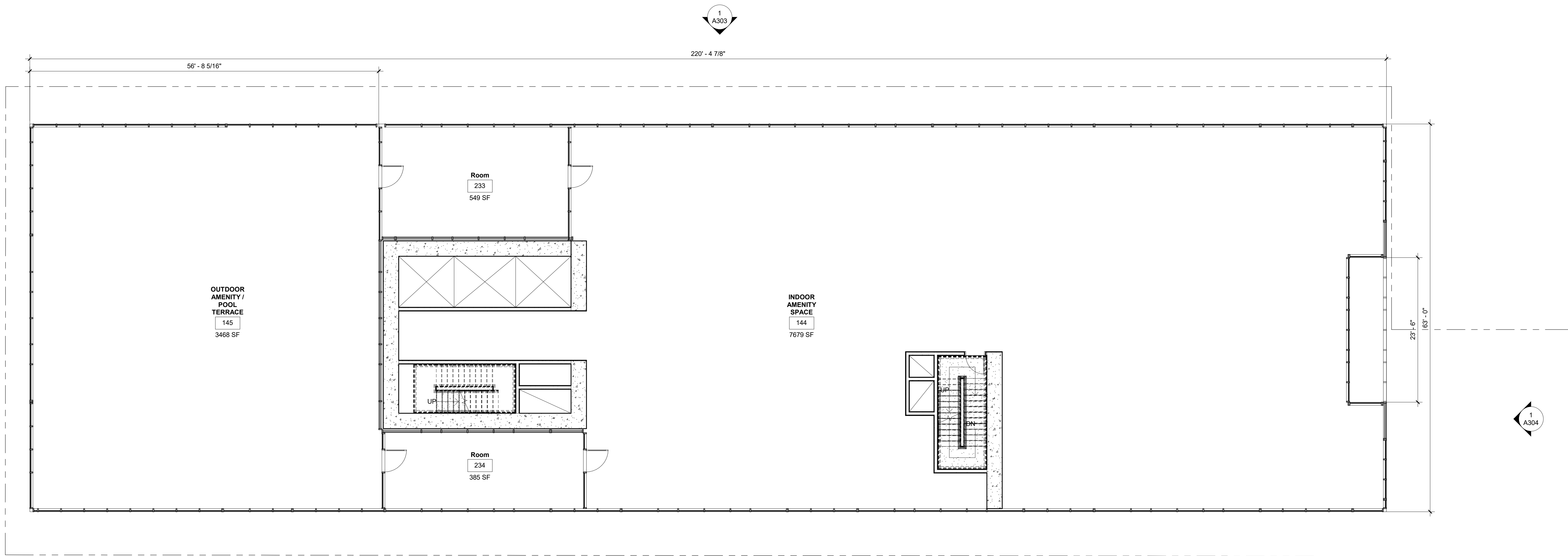
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

LEVEL 24
AMENITY FLOOR
PLAN

A124





RYAN A+E, INC.
533 South Third Street, Suite 100
Minneapolis, MN 55415
612-492-4000 tel
612-492-3000 fax

WWW.RYANCOMPANIES.COM

OWNER

EXCELSIOR
The Excelsior Group

CONSULTANTS

KEY PLAN

PROJECT INFORMATION

**BLOCK ONE
RESIDENTIAL**

710 S 4TH STREET
MINNEAPOLIS MN,
55415

© 2018 RYAN A+E, INC.

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 12/19/2018

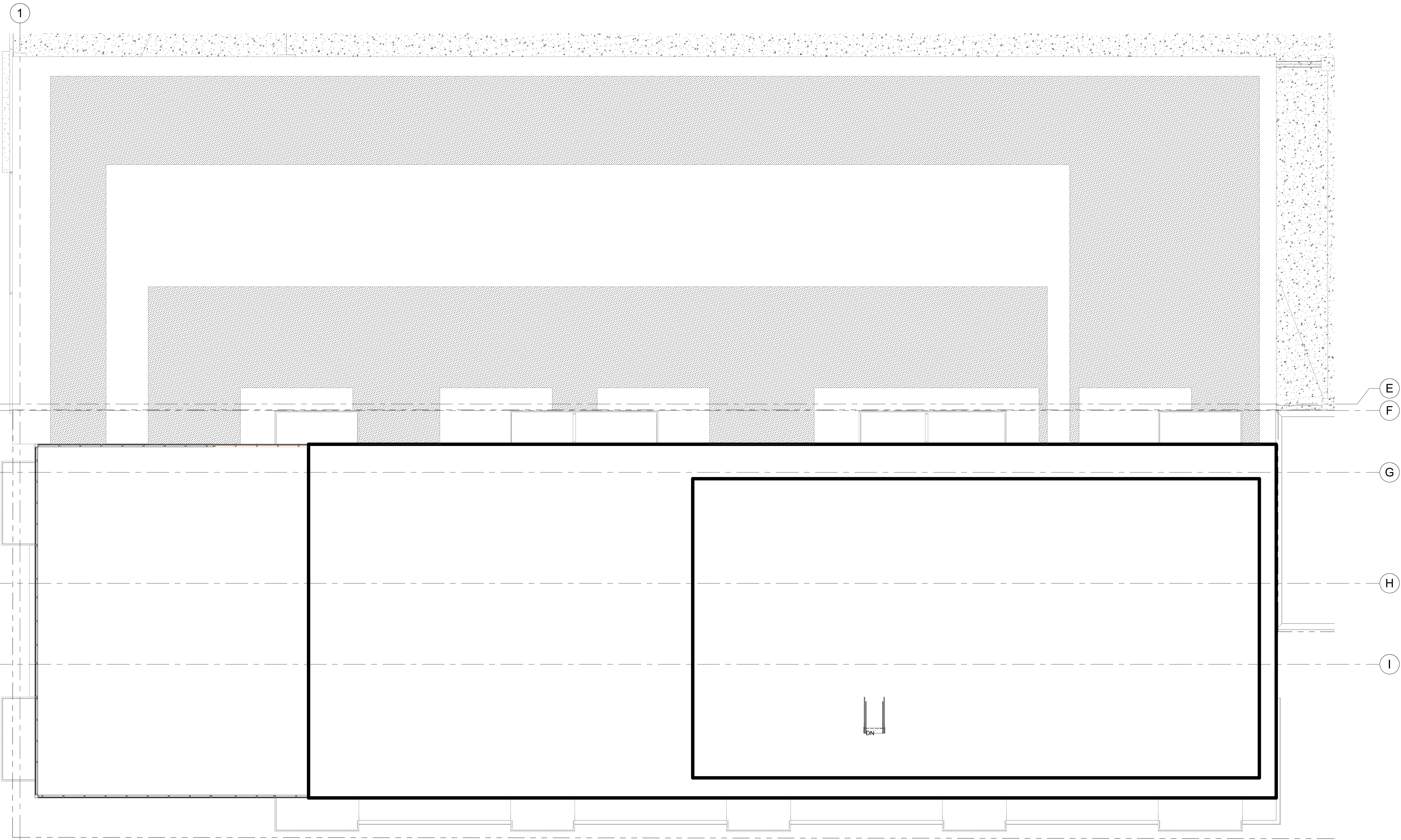
ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

LUA SUBMITTAL
DECEMBER 19TH, 2018

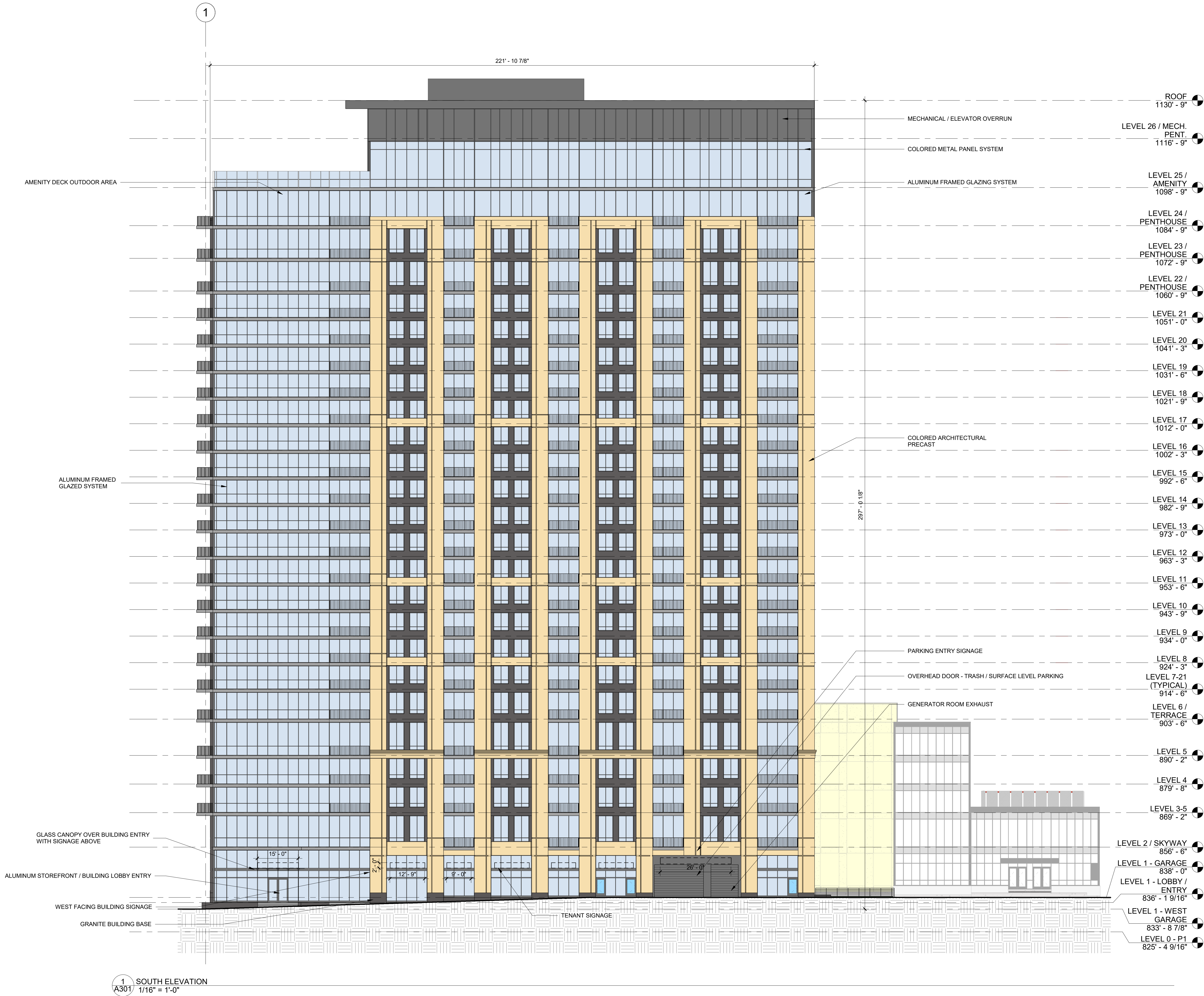
ROOF PLAN

A125

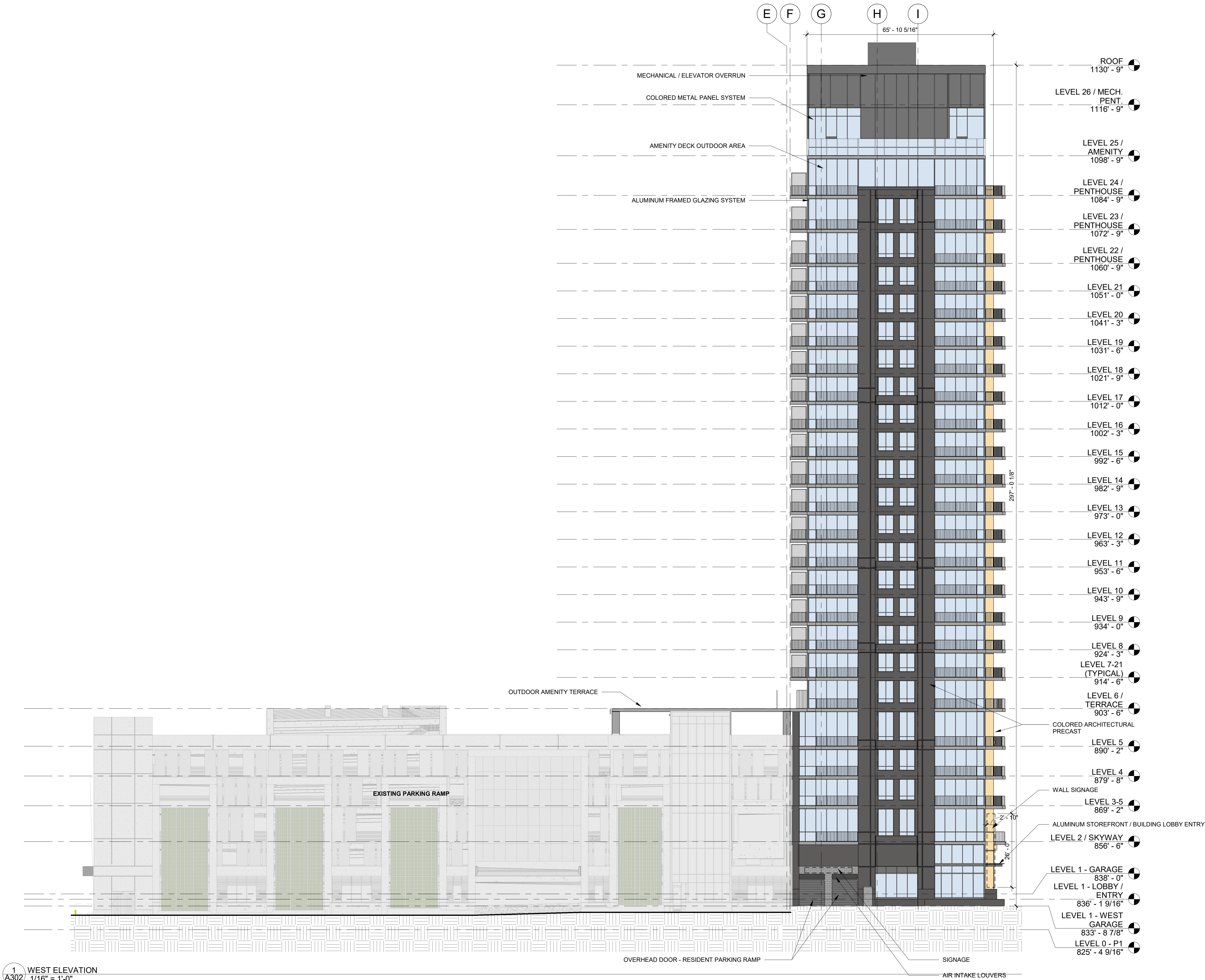


1 ROOF
A125 1/8" = 1'-0"

SOUTH ELEVATION - EXTERIOR MATERIAL QUANTITY		
PRECAST	20,775 SF	33%
METAL PANEL	6,783 SF	11%
GLASS	35,850 SF	56%
TOTAL SF	63,408 SF	



WEST ELEVATION - EXTERIOR MATERIAL QUANTITY		
PRECAST	5,656 SF	30%
METAL PANEL	3,344 SF	16%
GLASS	10,950 SF	54%
TOTAL SF	19,950 SF	



BLOCK ONE
RESIDENTIAL

710 S 4TH STREET
MINNEAPOLIS MN,
55415

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 02/01/2019

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

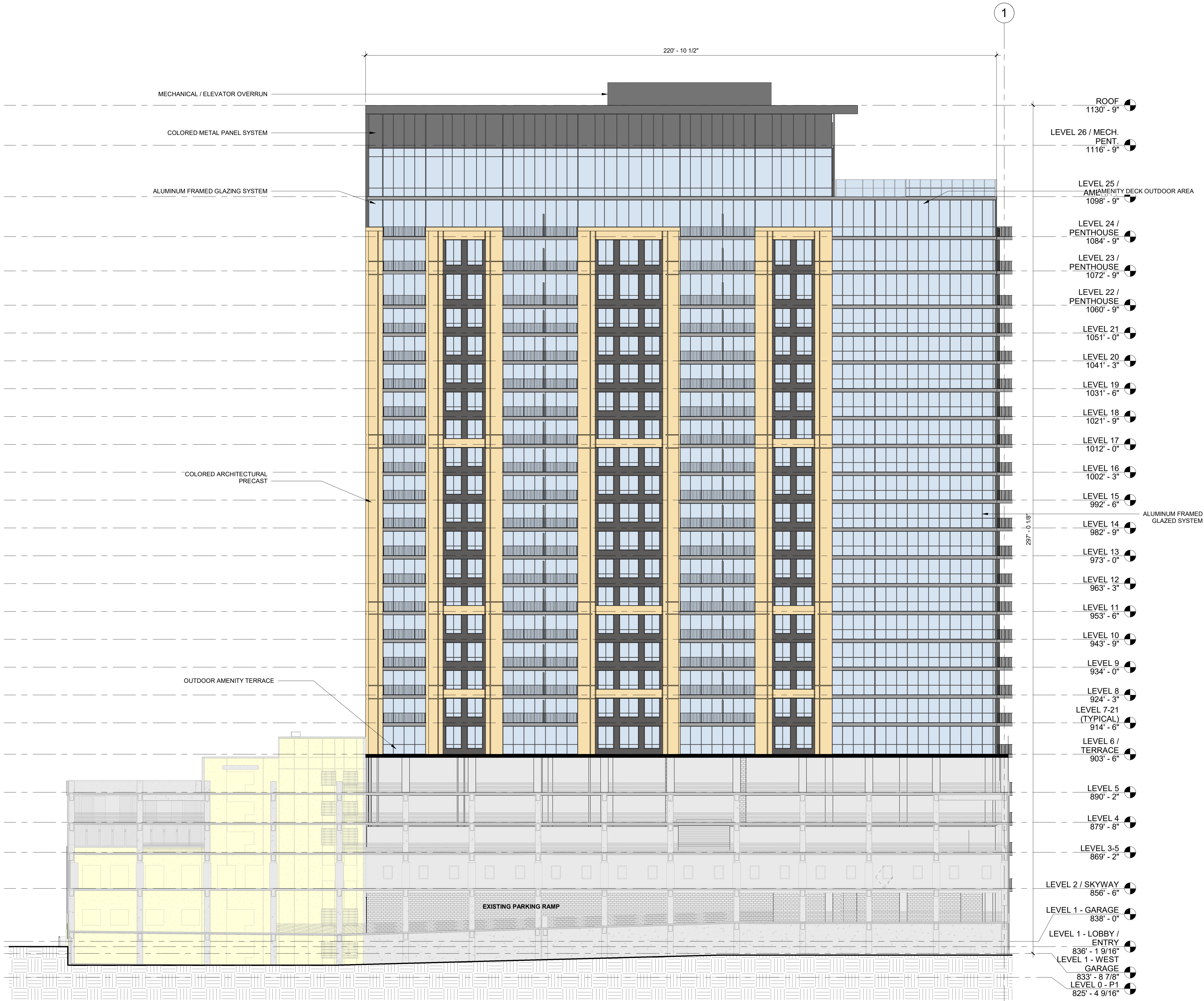
LUA
RESUBMITTAL

FEBRUARY 1ST, 2019

EXTERIOR
ELEVATIONS

A303

NORTH ELEVATION - EXTERIOR MATERIAL QUANTITY		
PRECAST	13,150 SF	27%
METAL PANEL	6,540 SF	14%
GLASS	29,040 SF	59%
TOTAL SF	48,730 SF	



BLOCK ONE
RESIDENTIAL

710 S 4TH STREET
MINNEAPOLIS MN,
55415

DRAWN BY CRC	CHECKED BY JRE
JOB NO. 700-515	DATE 02/01/2019

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

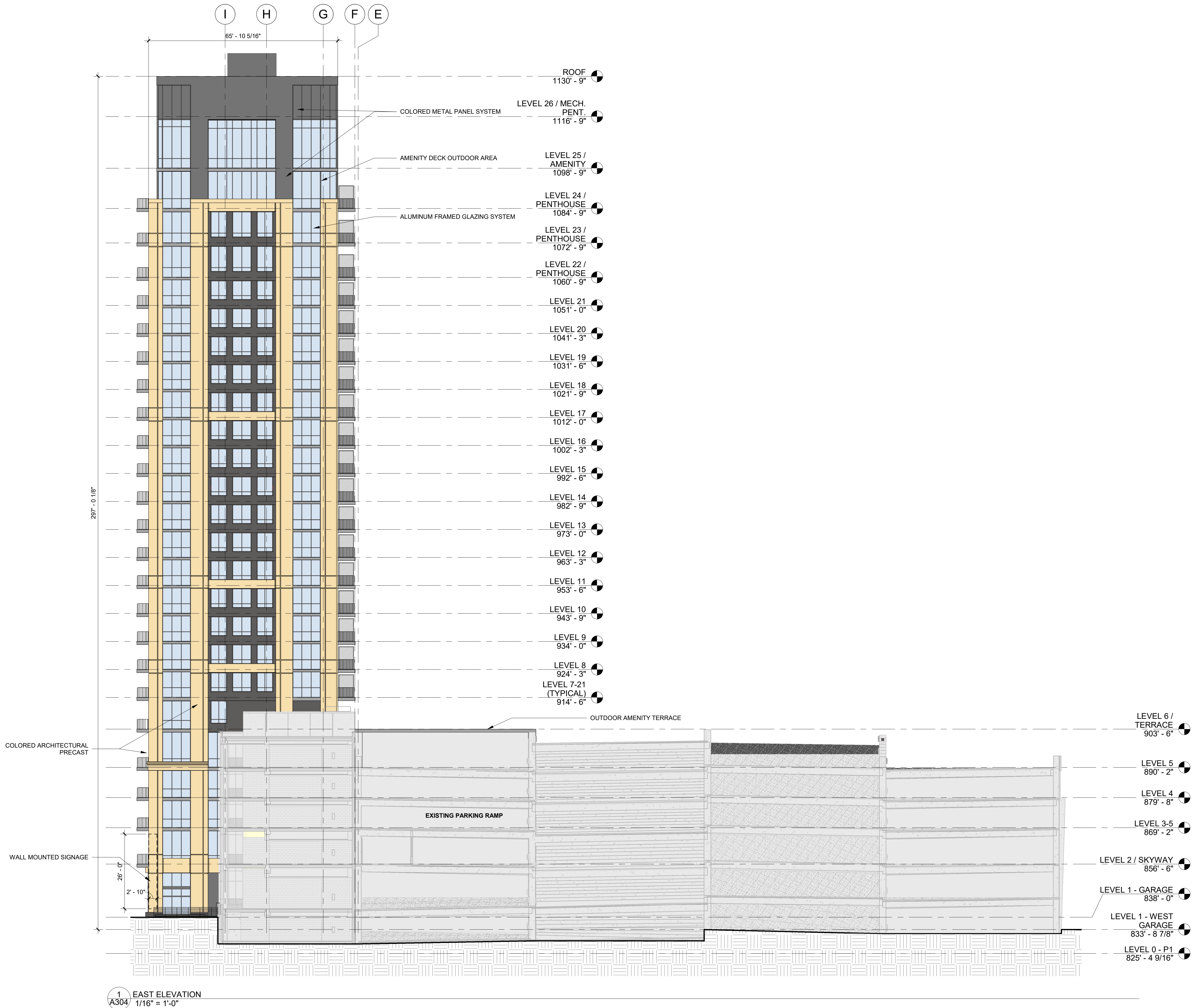
PRELIMINARY
NOT FOR CONSTRUCTION

LUA
RESUBMITTAL

FEBRUARY 1ST, 2019

EXTERIOR
ELEVATIONS

EAST ELEVATION - EXTERIOR MATERIAL QUANTITY		
PRECAST	7,487 SF	45%
METAL PANEL	1,919 SF	12%
GLASS	6,885 SF	42%
TOTAL SF	16,291 SF	





December 19, 2018

City of Minneapolis – Ward 3

Attn: Council Member Steve Fletcher
David Zaffrann, Ward 3 Senior Policy Aide

350 South 5th Street, Room 307

Minneapolis, Minnesota 55415

612-673-2203

steve.fletcher@minneapolismn.gov

**RE: Downtown East Development Phase V – Block 1 Residential
710 South 4th Street
Notice of Land Use Applications**

Council Member Fletcher and David,

We are incredibly excited to submit our Land Use Application today for Phase V of our Downtown East development, a residential mixed-use tower on the northeast side of the Commons Park. The site is located at the northeast corner of the intersection of Park Ave & 4th Street and is currently a vacant grass parcel located immediately to the south of the Mills Fleet Farm Parking Ramp, which was built in the first phase of the Downtown East development. The proposed development will consist of a 25-story (24 occupiable) mixed-use tower with 1,070 square feet of commercial space, 342 residential rental units and 120 parking stalls.

The purpose of this letter is to provide formal notice that we are submitting the following Land Use Applications:

- Conditional Use Permit for Phase V of a PUD
- Site Plan Review
- Subdivision

Please feel free to reach out to me anytime if you have any questions.

Respectfully,

A handwritten signature in blue ink, appearing to read "JP", with a stylized flourish at the end.

Joseph Peris
Real Estate Development Manager
Ryan Companies US, Inc.
612-492-4183
Joseph.Peris@ryancompanies.com

Ryan Companies
533 South Third Street, Suite 100
Minneapolis, Minnesota 55415

ph: 612-492-4000
ryancompanies.com





December 19, 2018

Downtown Minneapolis Neighborhood Association
Attn: Christie Rock Hantge, DMNA
Joe Tamburino, Chair of DMNA Board
Kevin Frazell, Chair of DMNA Land Use Committee
40 South 7th Street, Suite 212 PMB 172
Minneapolis, Minnesota 55402
320-583-4573
christie@thedmna.org



RE: **Downtown East Development Phase V – Block 1 Residential**
710 South 4th Street
Notice of Land Use Applications

Christie, Joe, and Kevin:

Ryan Companies and The Excelsior Group appreciate the DMNA's ongoing engagement on Phase V of our Downtown East development. We are incredibly excited to submit our Land Use Application today in anticipation of a Q1 2019 Planning Commission hearing. The site is located at the northeast intersection of Park Ave & 4th Street and is currently a vacant grass parcel located immediately to the south of the Mills Fleet Farm Parking Ramp, which was built in the first phase of the Downtown East development. The proposed development will consist of a 25-story (24 occupiable) mixed-use tower with 1,070 square feet of commercial space, 342 residential rental units and 120 parking stalls.

The purpose of this letter is to provide formal notice that we are submitting the following Land Use Applications:

- Conditional Use Permit for Phase V of a PUD
- Site Plan Review
- Subdivision

Please feel free to reach out to me anytime if you have any questions.

Respectfully,

Joseph Peris
Real Estate Development Manager
Ryan Companies US, Inc.
612-492-4183
Joseph.Peris@ryancompanies.com

Ryan Companies
533 South Third Street, Suite 100
Minneapolis, Minnesota 55415

ph: 612-492-4000
ryancompanies.com



40 S. 7th Street, STE 212 PMB 172
Minneapolis, Minnesota 55402
Phone: (612) 659-1279
Online: www.thedmna.org

February 15, 2019

Hilary Dvorak, Principal City Planner
City of Minneapolis CPED
250 South 4th Street, Room 300
Minneapolis, MN 55415

Re: Ryan Companies – 700 S. 4th Street

Dear Hilary:

I am writing to you on behalf of the Downtown Minneapolis Neighborhood Association Ryan Companies residential project proposed for 700 S. 4th Street. Joseph Peris from Ryan Companies presented the project to the DMNA Land Use Committee on November 13, 2018, and again on January 8, 2019.

Peris explained that the project is the final phase of Ryan Companies overall Downtown East redevelopment plan. He stated that the site is located at the corner of 4th Street and Park Avenue, or south of the Mills Fleet Farm Parking Ramp, overlooking The Commons. He described the project as a 25-story, 345 unit mixed-use residential building with 120 parking stalls and over 250 bike parking stalls. The site is ideally located near US Bank Stadium's LRT Station and the city's trail system, which should give residents the option to not have a vehicle.

Peris then reviewed the site plan and architectural drawings. He explained that the design is an intentional blend of modern and traditional and will have great views of the downtown skyline, the Mill District, and the river. He stated that the tower includes a mix of units including micro, studio, one and two-bedroom apartments and penthouses. He noted that amenities include, a rooftop pool deck, pet exercise area, bike kitchen & lockers, and multiple outdoor amenity areas. The building will connect to the city's skyway system.

Peris indicated that they updated the exterior design since their first presentation to the Land Use Committee in December. Based on the committee's strong recommendation, they scaled back the cap of the building. In addition, they hired a lighting consultant to create a lighting plan. Peris also noted that the landscaping plan was just preliminary and that there are more details to come. They will come back to the Land Use Committee to show the final drawings.

Peris concluded his presentation by noting that Ryan Companies submitted the land use applications associated with this project to the City in December. The land use applications include: a CUP for Phase 5 of a PUD, a Site Plan Review, and a Site Subdivision. He said that they anticipate presenting to the Planning Commission in February. He indicated that they hope to start construction in mid-2019.

After some questions and discussion, the Land Use Committee recommended providing a letter of support for Ryan Companies residential project proposed for 700 S. 4th Street. The committee has a strong interest in the public realm and wants to see the developer meet or exceed the guidelines established in the Downtown Public Realm Framework. The committee also suggested doing something to conceal the pet exercise area from the street view to make it look less obvious that it is a dog run. The committee also discouraged the use of benches in the public realm, due to the fact that skateboarders seem to do a lot of damage to them. The DMNA Board reviewed and supported the Land Use Committee's recommendation at their meeting on January 28.

If you have any questions regarding this letter, please feel free to contact me at christie@thedmna.org, or 320-583-4573.

Kind regards,

Christie Rock Hantge
DMNA Neighborhood Coordinator

CC: Joseph Peris, Ryan Companies
Council Member Steve Fletcher
Joe Tamburino, DMNA Board Chair
Kevin Frazell, DMNA Land Use Committee Chair

Dvorak, Hilary A.

From: Jamie Vicha <jmv55413@gmail.com>
Sent: Monday, March 04, 2019 12:37 PM
To: Dvorak, Hilary A.
Subject: Downtown East Phase V, Block One Residential Tower, 700 South 4th Street

Dear Ms. Dvorak:

I am wondering about the status of this project and if it has been approved. I hope you will forward my comments below to the planning commission:

Development should not include any additional parking - residents should have access to the parking garage it's attached to!

Street level should include retail! If Mpls wants to develop a more active street life, and not force people to drive out to the suburbs - new developments should include retail space to serve the new residents, office works and visitors/tourists (this development is right next to the stadium, park, as well as other cultural attractions). More restaurants and service retail space is needed - drug store, drycleaners, etc.

Thank you.

Jamie Vicha